

**Appendix E6  
SWG-3 Meeting**

**SUBJECT** Stakeholder Working Group (SWG) Meeting No. 3  
Aesthetic themes and elements, what to carry forward for  
additional analysis

**PROJECT** Feasibility Study for Arlington Avenue Bridges Replacement

**LOCATION** Remote Zoom Teleconference

**DATE/TIME** Tuesday, December 15, 2020, 1:00 - 3:15 p.m.

**MODERATOR** RTC Project Manager Judy Tortelli

#### INVITATION

- Zoom meeting conference call invitation from RTC Project Manager Judy Tortelli

#### PREPARATION

- PowerPoint presentation distributed via email:
  - overview of downtown bridges, conditions and planned replacements
  - photographic examples of aesthetic elements
- recaps of SWG-1 and SWG-2 and TAC-1 and TAC-2 meetings

#### ATTENDANCE

- 29 (see Zoom log-ins attachment), including:
  - 3 area residents
  - 7 representing the City of Reno
  - 6 representing community organizations
  - 1 representing the Reno-Sparks Indian Colony
  - 1 representing the Carson Truckee Water Conservancy
  - 1 representing Nevada Department of Transportation (NDOT)
  - 1 representing the Federal Highway Administration (FHWA)
  - 9 representing RTC (project management) and design and outreach subcontractors

#### MEETING NOTES

Taken by court reporter Brandi Smith, Litigation Services, and provided as a pdf.

#### WELCOME - RTC Project Manager Judy Tortelli

- welcomed everyone and introduced herself
- introduced two project team members who assisted with the presentation
  - Barb Santer, Stantec Senior Landscape Architect
  - Mike Cooper, Jacobs Structural/Bridge Engineer
- introduced Brandi Smith, court reporter from Litigation Services
- previewed the agenda, asking that questions be held for breaking points
  - review Arlington Avenue Bridges-specific history, downtown bridges' existing conditions, and design goals, opportunities and constraints
  - presentation on eight aesthetic elements in three categories (Barb Santer)
  - presentation on recommended bridge concepts (Mike Cooper), which affect aesthetic elements
  - group discussion and consensus
- Barb Santer and Mike Cooper re-introduced themselves

#### PRESENTATION INTRODUCTION - RTC Project Manager Judy Tortelli

##### SWG-3 MEETING PURPOSE

- discuss aesthetic elements for the project
  - presented at a high level to maintain some flexibility
- eight proposed aesthetic elements in three categories
  - overall theme and lighting
  - railing and widening sidewalk space
  - surface textures
- determine what to carry forward for additional analysis
  - in preparation for the next phase of the project: NEPA and design

##### BACKGROUND/HISTORY

- aesthetic theme possibilities identified at the beginning of the project
  - following the Downtown Streetscape Master Plan
  - mimicking the Virginia Street Bridge
  - "family of bridges" for this project and future replacements
  - developing a Wingfield Park-specific theme
- feedback at the end of the SWG-2 meeting
  - the Downtown Streetscape Master Plan doesn't specifically address bridges and would limit flexibility
  - a "family of bridges" standard could be helpful as there will be several downtown replacements in the near future
- preliminary theme agreed upon: modern design elements with a nod to the art deco historical context

PRESENTATION continued - RTC Project Manager Judy Tortelli

#### “FAMILY” OF DOWNTOWN BRIDGES

- map showing bridge locations (presentation slide 2) from west to east: Booth Street, Keystone Avenue, Arlington Avenue, Sierra, Virginia, Center and Lake Streets
  - Virginia Street Bridge recently replaced
  - Arlington Avenue, Keystone Avenue and Sierra Street Bridges to be replaced (RTC 2040 Regional Transportation Plan) between 2022 and 2026
  - Lake Street Bridge to be replaced between 2027 and 2040 (scheduled later to address flooding after “band-aid” installed in 2020)
- with four upcoming bridge replacements, the “family of bridges” aesthetic theme looks appealing and achievable

#### PRESENTATION AESTHETIC ELEMENTS - Barb Santer, Stantec Senior Landscape Architect ARLINGTON AVENUE BRIDGES-SPECIFIC STUDY

- SITE HISTORY HIGHLIGHTS (presentation slide 3)
  - Wingfield Park has been an important Reno park since its development as Belle Isle (1909) with a resort-like atmosphere and amusement rides
  - the north bridge structure, Nevada’s oldest T-beam bridge was constructed in 1912
  - Banker George Wingfield bought the property and donated it to the city in 1920
  - natural beauty of Wingfield Park illustrated in the 1945 novel, *The City of Trembling Leaves*, by Walter Van Tilberg Clark
  - potential to connect the aesthetic elements to the site’s history and natural beauty
- ARCHITECTURAL CONTEXT OF EXISTING DOWNTOWN BRIDGES AND RIVERWALK
  - Booth Street Bridge (presentation slide 4): single-pier type with smooth abutment wall; riprap for slope reinforcement; smooth, solid concrete railing; globe accent lighting; textured, solid concrete wall with a sidewalk; pedestrian access to Idlewild Park
  - Keystone Bridge: different style (no details)
  - North and South Arlington Avenue Bridges (presentation slides 5 and 6) - texture created by boulders added for kayak park; view of river pools and riffles (wildlife, Reno’s largest swimming hole)
  - North Arlington Avenue Bridge (presentation slides 5 and 6): two piers; natural rock texture, transitioning to solid concrete floodwalls; sidewalk adjacent to solid concrete barrier rail; pedestrian access underneath; riprap added with kayak park; lights at either end removed but not replaced after a flood
  - South Arlington Avenue Bridge (presentation slides 5 and 6): solid, smooth concrete railing panels; pilasters; view of the Riverwalk; view of the gates for kayak competitions



## PRESENTATION AESTHETIC ELEMENTS - Barb Santer continued

- ARCHITECTURAL CONTEXT OF EXISTING DOWNTOWN BRIDGES AND RIVERWALK
  - Sierra Street Bridge (presentation slide 7): double, smooth-textured piers; smooth-textured flood wall; concrete railing with openings (meeting current accessibility requirements) and pedestrian-scaled lights; art-deco-look pilasters; sidewalk adjacent to railing; railing design and globe lights continue along floodwall
  - Virginia Street Bridge (presentation slide 8): recently replaced/reopened; clear-span (tied arch); large concrete arches that have a layered-concrete-look art-deco feature at the end; widened (bowed-out) deck for more pedestrian gathering space; double (for vehicle protection) dark gray, powder-coated railing in arch design that ties into the bridge arches; railing features stainless steel top and built-in lighting; four-lights clusters at four corners, salvaged from historic bridge; night lighting of the railings and arches, concrete and brick sidewalk paving with some tie-in to earlier Downtown Streetscape standards (not current)
  - South Center Street Bridge (presentation slide 9): replaced circa 1996; art deco design in context of the nearby downtown Post Office (chevron features); double railing due to widened bridge deck (for pedestrian viewing area), transparent art-deco top on the outside and concrete vehicular-rated on the inside with globe lights
  - Lake Street Bridge (presentation slide 10): may be replaced in 2027 timeframe; more utilitarian, removable metal railing due to flooding; four decorative lights (similar to the Sierra Street Bridge) on pilasters
  - Riverwalk (presentation slide 11): two segments, first, between Virginia and Sierra Streets, completed in early 90s; dark cherry powder-coated metal railing in several designs; dark cherry metal also used for pergola caps; stainless chain and bollard barriers; natural stone veneer on floodwall with natural granite cap on concrete; pedestrian-scaled lights; benches under arbors; custom granite pedestrian surfaces; dark cherry pedestrian bridge to Wingfield Park; treatment extended in second phase from Sierra Street to Arlington Avenue, pedestrian surfaces replaced with stamped, ashlar concrete
- PUBLIC ART PROGRAM CONTEXT (presentation slide 12)
  - heart of the arts and culture district
  - pieces at Bicentennial Park, the Nevada Museum of Art and City Plaza
  - murals throughout downtown
- ARLINGTON AVENUE BRIDGES SITE INVENTORY (presentation slide 13)
  - plan view map shows two bridges and surrounding points of interest, including Wingfield Park, West Street Plaza and Barbara Bennett Park
  - other context illustrated by photos: art at Bicentennial Park, the kayak park, under-bridge pathway, sports courts, a mural, the Wingfield Park amphitheater, street trees, and one or three nearby pedestrian access/bridges

#### PRESENTATION BRIDGE CONCEPTS - Mike Cooper, Jacobs Structural/Bridge Engineer

- REVIEW OF THREE RECOMMENDED CONCEPTS after TAC-1 and TAC-2 and SWG-1 and SWG-2 meetings
  - clear span rigid-frame structure (presentation slides 15-17)
    - | advantages: no obstructions in the north branch of the Truckee (currently two supports in the river) and no surfaces for tagging
    - | challenges: sidewalk expansion due to thin section at mid-span, less headroom for pedestrian under-bridge access
    - | rendering showing clear span over the river (no in-channel supports) and under-bridge pathway in Wingfield Park
  - two single-pier (center), two-span concepts (presentation slides 18-20)
    - | cast-in-place, concrete box girder (solid slab from underneath)
    - | precast concrete girders (placed during construction, individual lines from underneath)
    - | similar advantages for both: can support a wider deck (sidewalk expansion), shallower structure that will help mitigate roadway profile changes and provide more headroom for under-bridge pathway
    - | similar challenges for both: one obstruction in the river, pier wall tagging surface
    - | renderings

#### PRESENTATION AESTHETIC ELEMENTS - Barb Santer continued

- OPPORTUNITIES AND CONSTRAINTS (presentation slide 14)
  - plan view map shows elements for analysis: pedestrian access areas including three foot bridges and below the north bridge, bicycle access, deteriorating flood walls (possible replacement, form liner patterns, treatment consistency), Riverwalk connection to Wingfield Park, mismatched railings, potential street elevation adjustments (regarding) and replacement trees
  - plan view map indicates other considerations: views of the Truckee River, large grass areas, tree-lined pathways, river recreation, Wingfield Park amphitheater (premier summer destination especially during July Art Town), Whitewater Park, existing stone steps (difficult access), existing utility boxes (eyesore), street closure (between Island Avenue and West First Street) for pedestrian traffic and booths during multiple special events
- DESIGN GOALS developed from opportunities and constraints (presentation slide 21)
  - use cohesive design language with consistent form and design elements to unify the experience on and viewing the north bridge and south bridge and to establish a project theme for the bridge and landscape elements
  - enhance pedestrian experience
    - | with Arlington Avenue as an urban plaza (unified materials between sidewalk and street)
    - | by maintaining vantage points of the river and landscape
    - | with more shade trees; decorative lighting, railings and paving; some sculptural and artistic features

## PRESENTATION AESTHETIC ELEMENTS - Barb Santer continued

- DESIGN GOALS (presentation slide 21) continued
  - create contextual and historical relevance by using structural elements relevant to the urban context and by paying homage to Reno's history while representing a new age of bridge development
  - incorporate innovation and sustainability with low impact development (LID) strategies, such as infiltrating stormwater with permeable pavements or concrete pavers and using energy-saving LED lighting
- PROPOSED AESTHETIC ELEMENTS (presentation slide 24) to be presented individually for comments and questions
- FIRST TWO ELEMENTS presentation - Barb Santer
- modern design with a nod to art deco (presentation slide 25)
  - decorative elements focused on pedestrian lighting, railings, under-bridge lighting, sculptural elements, and possibly applied to under-bridge form liner, pilasters and girders
  - reference images: concrete design, grillwork, railings, pedestrian and bridge lighting
- lighting (presentation slides 26-28)
  - protected from vandalism, flood water and debris
  - taking into account how it affects aquatic species
  - pedestrian-scaled lighting on bridges
    - | examples from art deco to modern art deco to modern; fixed in pilasters or bridge posts, along walkway and in railings
    - | lighting elements could continue between north and south bridges to create unity
    - | globe light used on the Center Street Bridge is also along Truckee River Avenue from Bicentennial Park to Booth Street and on the north side flood wall
  - bridge accent lighting
    - | girder lighting to unify the north and south bridge
  - under-bridge lighting
    - | combined aesthetic and pedestrian safety
  - lighting concept that could be applied to other downtown bridges
  - aesthetic lighting experience viewing and walking along bridges
- FIRST TWO ELEMENTS discussion - moderated by Barb Santer
  - comment, Anne Buja, St. Thomas Aquinas. Loves the visually interesting idea of juxtaposing modern bridges with art deco theme. Question: how vulnerable to vandalism is under-bridge lighting?

- FIRST TWO ELEMENTS discussion continued - moderated by Barb Santer
  - comments, Kerrie Koski, City of Reno. Likes the idea of lighting emphasizing art deco elements. Believes under-bridge lighting could be designed to be easy to maintain but hard to vandalize. Would like to consider changeable lighting, i.e., different colors for different events. For on-bridge lighting, believes globe lights concept could carry through the “family of bridges.” On bridge structure, now prefers center pier to give the bridges character and allow wider sidewalks.
  - comments, Alex Stettinski, Downtown Reno Partnership. Thinks wider sidewalks are key for the flow of visitors to the neighborhood. Agrees with changeable colors for under-bridge lighting, could unite the look of the bridges at night. Emphasized that paths need to be well lit to make people feel safer. Believes the combination of history with a modern overlay will be “stunning.”
  - comments, John L’Etoile, Department of Plans and Architecture, NDOT. Agrees with the theme. Giving art deco a contemporary appeal will resonate. Cautions against creating a “wow” factor with lighting. Likes the idea of color to tie the area together, but with a subdued approach. Lighting that enhances the beauty of the structure or the natural features of the area and the river itself (the center piece). Prefers the clear-span bridge so the river can be seen from more vantage points.
  - comments, Father Durante, St. Thomas Aquinas. Loves the modern-art deco theme. Also prefers the clear-span (no center pier) bridge. Broader sidewalks may not be needed because the street is often closed to cars for events. Question: will the walls (railings) along the sidewalks be high enough so (inebriated) guests won’t fall off the bridge?
  - response, Barb S., Stantec (confirmed by Mike Cooper, Jacobs). 42 inches tall is the current requirement for the bridge railings.
  - comments, Greg Erny, Architects Plus. Let’s enhance pedestrian access to Wingfield Park and the island. Likes the idea of lighting but agrees that it should be subtle, focusing on the features of the area not the light source. LED lighting accommodates color changes. Prefers the clear-span bridge to enhance views from above and from the river. Would like design to allow utilities (or other items in the future) to cross the river within the bridge.
  - comment, Anne B., St. Thomas Aquinas. Looking at other bridges, I found lighting on the outside that frames the structure to be really attractive, as opposed to underneath that could draw the eye to parts we don’t want to emphasize.
  - comments Michon Eben, Reno-Sparks Indian Colony. Lighting should be limited for the natural rhythm of the surroundings. Question: can there be a historical marker recognizing the connection between the Truckee River and native, indigenous culture? Maybe this is for the NEPA document.
  - comment, Matt Brezina, City of Reno Parks Department. Walking path underneath the bridge is an important aspect of the park plan and the bridge plan. Height should be taken into account in relation to bridge style.

- FIRST TWO ELEMENTS discussion continued - moderated by Barb Santer
  - question, Kerrie K., City of Reno. Can someone speak to that in general terms? Also sidewalk widths and rail height with or without a center pier?
  - response, Mike C., Jacobs. In preliminary layouts, it's eight-foot sidewalks across the bridges, which could be widened to 12 feet or more with center pier providing support for the wider area. The rigid-frame, clear-span structure gets support from thickened ends/abutments. How much headroom that takes up and what the grades would look like would be part of further studies.
  - question, John L., NDOT. Could you get a 10-foot-wide sidewalk with a clear-span?
  - response, Mike C., Jacobs. It's possible by thickening a section to support the sidewalk, but the idea of the clear-span is to minimize the structure depth over the river.
  - comment, Barb S., Stantec. Eight feet with a clear-span is not bad considering that standard commercial width would be five feet.
  - question, John L., NDOT. If there is no grading between the bridges, would there be an opportunity to look at the point of connection between the pedestrian walks and the sidewalk along Arlington?
  - response, Judy T., RTC. Once we determine what the bridge footprint will be, we'll look at all connections into existing pathways, a key project component. Right now, we're not sure.
- SECOND SERIES OF ELEMENTS presentation - Barb Santer
- railings (presentation slide 29)
  - important to establish some transparency in the design
  - public request to be able to see the river on Virginia Street and Center Street Bridge replacements
  - design possibilities
    - | cutouts in concrete railing
    - | vehicular-rated metal guardrail with chevron art deco detail
    - | hybrid of concrete railing with cutouts on the bottom and transparent railing on top; may want to avoid this double railing to make it easier to walk back and forth across the street during special events
- maintain pedestrian accessibility (presentation slide 30)
  - avoid double railing like Center Street Bridge
  - if Arlington Avenue elevation needs to be raised, regrade those areas into the park, making grass areas slightly steeper; no small retaining walls that would interrupt pedestrian movement
  - raising Arlington Avenue elevation/regrading could be an opportunity for street tree planting
  - provide graffiti coating for maintenance/easy removal (protection with accessibility)
  - create a nice pedestrian space

- SECOND SERIES OF ELEMENTS presentation continued - Barb Santer
- widened bridge deck (presentation slide 31)
  - to allow for greater pedestrian viewing
  - needs bridge with single pier in the river
    - clarification from Mike C, Jacobs: a wider sidewalk over the full length of the clear-span, rigid-frame structure could be accommodated; may cause other conflicts
  - Center Street Bridge and Virginia Street Bridge have widened segments
- SECOND SERIES OF ELEMENTS discussion - moderated by Barb Santer
  - questions
    - | do we want the transparent railings?
    - | should we avoid double railings to maintain smooth pedestrian movement?
    - | should the bridge deck be widened?
  - comments, Kayla Dowty, Carson-Truckee Water Conservancy District (local sponsor for 408 permit). Cantilevered sidewalks and cable railing (like Virginia Street Bridge and Center Street Bridge) allow virtually no large equipment access to the river from the bridge to mitigate flood impacts (clear debris) or for general maintenance. Potential problem for 408 permit application. Another way for the City of Reno to access the river to remove sediment buildup from kayak park features would also be beneficial.
  - response, Judy T., RTC. Thanks to Kayla D. Project team will be keeping access in mind as they determine what the bridge footprint will be.
  - comments, Theresa Jones, City of Reno. Piggybacking on Kayla D.'s comment, NDOT's very large under-bridge inspection trucks (UBIT) will also need access. Also assuming that bridge is being modeled and designed to pass the 100-year flood event.
  - response, Mike C., Jacobs. A thinner structure might do better at providing flood capacity and have less impact on the profile of the roadway above. Profiles are constrained at both ends of the project: north bridge and south bridge tie into intersections, cannot step on adjacent properties.
  - comments, Anne Buja, St. Thomas Aquinas. The single railing helps to set the Virginia Street Bridge apart from other bridges with lower-profile railings. Likes visually opening up the railings (transparent).
  - comments, John L., NDOT. Transparent railings for sure. For safety, instead of double railings, maybe an aesthetic pilaster or bollard so that pedestrians can move back and forth easily but a car couldn't come up on the sidewalk. On widening the bridge deck, possibly closer to the abutment (two or four corners) where the structure is more robust.
- THIRD SERIES OF ELEMENTS presentation - Barb Santer
- textured abutment walls (presentation slide 32)
  - as opposed to flood walls, abutment walls are right underneath on the only downtown bridge with pedestrian access below it
  - currently smooth; texture would enhance pedestrian/river-user experience



- THIRD SERIES OF ELEMENTS presentation continued - Barb Santer
- textured abutment walls (presentation slide 32) continued
  - myriad of possible textures
    - | cobble texture already being used on the Riverwalk and oldest flood walls
    - | custom, stylized “form liner”
    - | natural cotton tree bark look
    - | repeated, stylized element, i.e., the Greek key from the downtown post office
- flood walls (presentation slide 33)
  - not a lot of replacement in this project
  - variety of flood walls in downtown, most common smooth concrete with pilasters and globe art deco lights
  - there should be some consistency between bridges
  - textures, horizontal reveals, railing, pilasters and lighting should also be consistent
  - south flood wall texture could match cobble along the Riverwalk
  - narrow planting along the base of the walls could soften the height, but may not be ideal from the engineering or maintenance side
- plaza street (presentation slide 34)
  - concept could unify park areas if the street between bridges needs to be replaced
  - unify paving, i.e., permeable pavers that would create a seamless transition from street to sidewalk and provide stormwater infiltration
    - | one paver style already on the northwest corner of the existing Arlington Avenue bridge
  - create a theme with a unique stamp or sandblast for sidewalk paving
- THIRD SERIES OF ELEMENTS discussion - moderated by Barb Santer
  - questions
    - | textured or smooth concrete abutment walls?
    - | for flood walls, should there be design consistency between bridges?
    - | what should north side flood walls look like? and south side?
    - | should we create a plaza street if the street between the bridges needs to be replaced?
  - comments, Kerrie K., City of Reno. From an engineer, on the plaza street concept, need to keep in mind that Arlington Is a street first and that textures shouldn't interfere with everyone's ability to access/navigate the plaza. Flood walls should be consistent, but also low maintenance.
  - comment, Matt Brezina, City of Reno Parks Department. Agrees with Kerrie K. Simple is better and easier, from color schemes to decorative textures that may have to be painted over due to graffiti.
  - comments, Anne Buja, St. Thomas Aquinas. Will what is done feel timeless? Similar to what NDOT has done along the highways, could we do local flora and fauna along the flood walls in a simple paint style that's easy to maintain?

- THIRD SERIES OF ELEMENTS discussion continued - moderated by Barb Santer
  - comment, Barb S., Stantec. Maybe reference the cottonwood or the trembling leaves, a significant component of the Truckee Meadows, an oasis in the middle of the Great Basin. Natural elements could tie in, too with Michon's (Eben, Reno-Sparks Indian Colony) thoughts about referencing the importance of the Truckee River to the tribes. Do it in a timeless, subtle way.
  - comment, Anne Buja, St. Thomas Aquinas. Not tacky.
  - comment, John L., NDOT. A little abstract so it resonates with the river, the cultural and the history of the Tribes.
  - comment, Anne Buja, St. Thomas Aquinas. Reference the Paiute people.
  - comments, John L., NDOT. On the patterns/textures, Greek symbol works better with an architectural element rather than the river. Agrees on keeping the walls similar throughout the river corridor. Deeper texture/pattern does impede graffiti.

#### CONSENSUS - RTC Project Manager Judy Tortelli

- everyone complimented Barb on an amazing presentation
- great discussion that indicates almost all proposed aesthetic elements should be carried forward for further review related to feasibility (finer details for NEPA and design)
  - exception: double railing won't be carried forward because it limits pedestrian accessibility
- call for additional ideas. Brian Stewart, RTC project team, suggested exploring the feasibility of a wider clear-span bridge, abutment to abutment, brought up by Mike C., Jacobs.

#### NEXT STEPS - Judy Tortelli

- summarize notes from two TAC meetings and three SWG meetings
- present findings and comments to the City of Reno Council and the RTC Board
- refine renderings of bridge concepts
- put together aesthetic elements to be shown for feedback at the second, final public meeting in February or March
- incorporate public feedback in the feasibility study and present to the City of Reno Council and RTC Board again before finalizing

#### PUBLIC COMMENT opened to non-SWG members - moderated by Judy Tortelli

- miscellaneous comments, Toni Harsh, area resident. Proportion of lights, related to whether they will be on pedestals or at street level, has been overlooked previously. Likes the idea of different motifs for the north side and south side flood walls, less repetitious, canyon feel. Addressing debris sweep upstream could clean up a lot of the situations with bridge structures downtown. Add ADA access to plan view map on the south side across from Barbara Bennett Park. Agrees that lighting should not be overwhelming. Consider shading issue (slippery areas) for pedestrian safety. Community wants to interact with the river.



PUBLIC COMMENT - moderated by Judy Tortelli

- miscellaneous questions and comments, Honor Jones, area resident. Wondered why elevated bridge concept was dropped and if it should be reconsidered. Could eliminate some issues, such as access for maintenance and debris removal. Would also mimic the Virginia Street Bridge elevated look and accommodate increasingly popular outdoor activities at Wingfield Park. And could prevent emergency vehicles from being caught by road closures/people in the street. Make sure flat surfaces are ADA compliant with no niches or grooves in which wheels can get stuck. Likes the increased lighting, should mimic the curve of the river and park.
- response, Judy T, RTC. Recap of SWG-2 meeting and TAC-2 meeting explain why the elevated bridge concept was not recommended to be carried forward. Will reach out to Ms. Jones when those are posted on the website.

CONCLUSION - Judy Tortelli and Barb Santer

- again great feedback today; got through a lot of material in a short time
- happy holidays to everyone

THANKS FOR PARTICIPATING (and reviewing this recap)

PROJECT WEB PAGE

- <https://www.rtcwashoe.com/engineering-project/arlington-avenue-bridges-project/>



Arlington Avenue Bridges Replacement  
**Stakeholder Working Group #3**  
**VIRTUAL MEETING AGENDA**



Tuesday, December 15, 2020

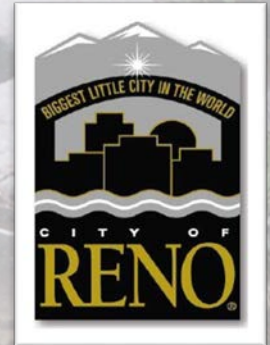
at 1:00 pm to 3:00 pm

Regional Transportation Commission

- ITEM 1      Introductions
  
- ITEM 2      Site History and Existing Conditions Inventory
  
- ITEM 3      Single Pier and Clear Span Bridge Concepts  
                 Pros/Cons for Aesthetics
  
- ITEM 4      Opportunities and Constraints Map
  
- ITEM 5      Design Goals
  
- ITEM 6      Proposed Aesthetic Elements
  
- ITEM 7      Group Discussion and Consensus
  
- ITEM 8      Public Comment
  
- ITEM 9      Adjournment



# Feasibility Study for



# ARLINGTON AVENUE BRIDGES REPLACEMENT

Stakeholder Working Group Meeting #3 |  
Aesthetic Themes | December 15, 2020



# Meeting Purpose

- ▶ Discuss aesthetic themes for the project
  - ▶ Initial Aesthetic Themes
    - ▶ Downtown Streetscape Master Plan
    - ▶ Virginia Street Bridge
    - ▶ “Family of Bridges” Theme
    - ▶ New - Wingfield Park Area
  - ▶ SWG-2 Input
- ▶ Recommended Bridge Concepts
- ▶ Help guide us
  - ▶ Which design elements need additional analysis
  - ▶ Input and ideas on appearance of the bridges

# Meeting Agenda

- ▶ Review site history, existing condition, and recommended bridge concepts
- ▶ Look at opportunities, constraints, and design goals
- ▶ Proposed Aesthetic Elements
  - ▶ Theme and Lighting  
Questions ??
  - ▶ Railing and Sidewalk Space  
Questions ??
  - ▶ Surface Texture  
Questions ??
- ▶ Group discussion and consensus

# Location Map: Downtown Bridges Reno, Nevada

**BOOTH STREET  
BRIDGE**

*PROJECT SITE*  
**ARLINGTON AVENUE  
BRIDGES**

**VIRGINIA  
STREET  
BRIDGE**

**LAKE STREET  
BRIDGE**



**KEYSTONE  
AVENUE BRIDGE**

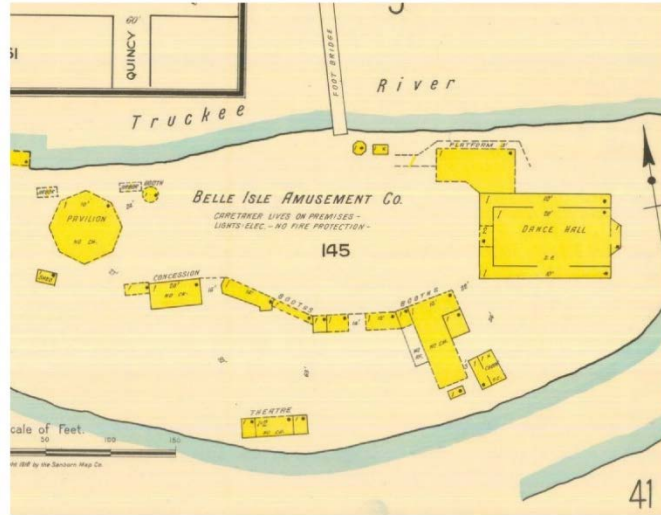
**SIERRA STREET  
BRIDGE**

**CENTER STREET  
BRIDGE**

--- FUTURE BRIDGE  
REPLACEMENT



# Site History



Belle Isle Map, 1918

- 1909 - Lewis Hinckley purchased and developed the land into a relaxing resort-like destination, known as Belle Isle
- 1912 - The river was partially dammed, and footbridge was added.
- 1916 - Owners of the park filed for bankruptcy and the space was acquired by Reno National Bank
- 1920 - Influential banker George Wingfield bought the property and donated it to the City. The island was later named "George Wingfield Park."
- 1921 - North bridge structure constructed, originally called Chestnut Street Bridge, Nevada's oldest T-beam bridge
- 1928 - A flood caused substantial damage to the park, with repair help by George Wingfield
- 1925-1980's - The Reno Municipal Christmas Tree stood at Wingfield Park



Electric Lights, 1911

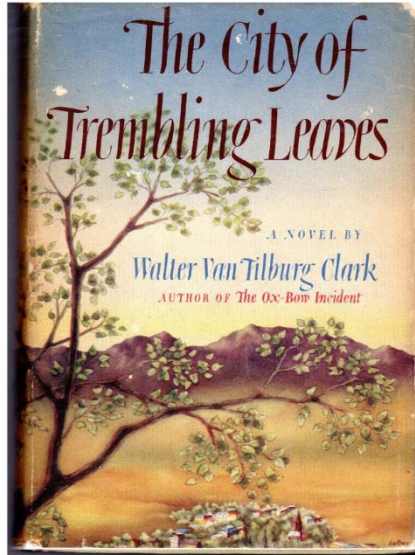


Belle Isle, 1911



Wingfield Park Postcard

# Design Inspiration: City of Trembling Leaves



- **The City of Trembling Leaves**, by Walter Van Tilburg Clark
  - The City of Trembling Leaves is a well-known book about the history and development of Reno
  - Reno was established as small town on the edge on the mountain range with beautiful trees and wildlife all around
  - The book illustrates the natural beauty of the Reno landscape and highlights Wingfield Park as a leisurely oasis within the City
  - Urbanization and development has changed the landscape of the City within the last century
  - How can the proposed design reconnect to its history and celebrate the natural beauty of the Reno landscape?



Cottonwood Trees in the Nevada Landscape



Cottonwood Tree Leaf



Truckee River through Reno



# Existing Conditions Inventory: Booth Street Bridge

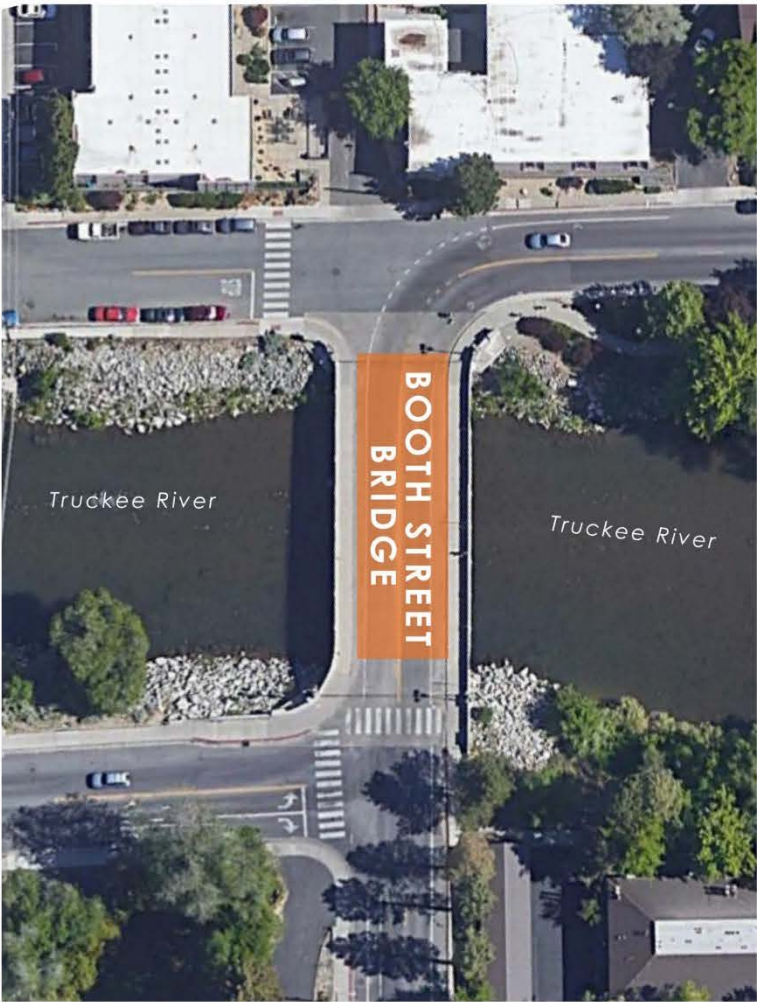
SINGLE PIER BRIDGE

SMOOTH TEXTURED ABUTMENT WALL

RIP RAP

TEXTURED SOLID CONCRETE BARRIER RAIL

PEDESTRIAN ACCESS



SITE PLAN



VIEW LOOKING SOUTH



VIEW LOOKING WEST



LIGHTING



SIDEWALK

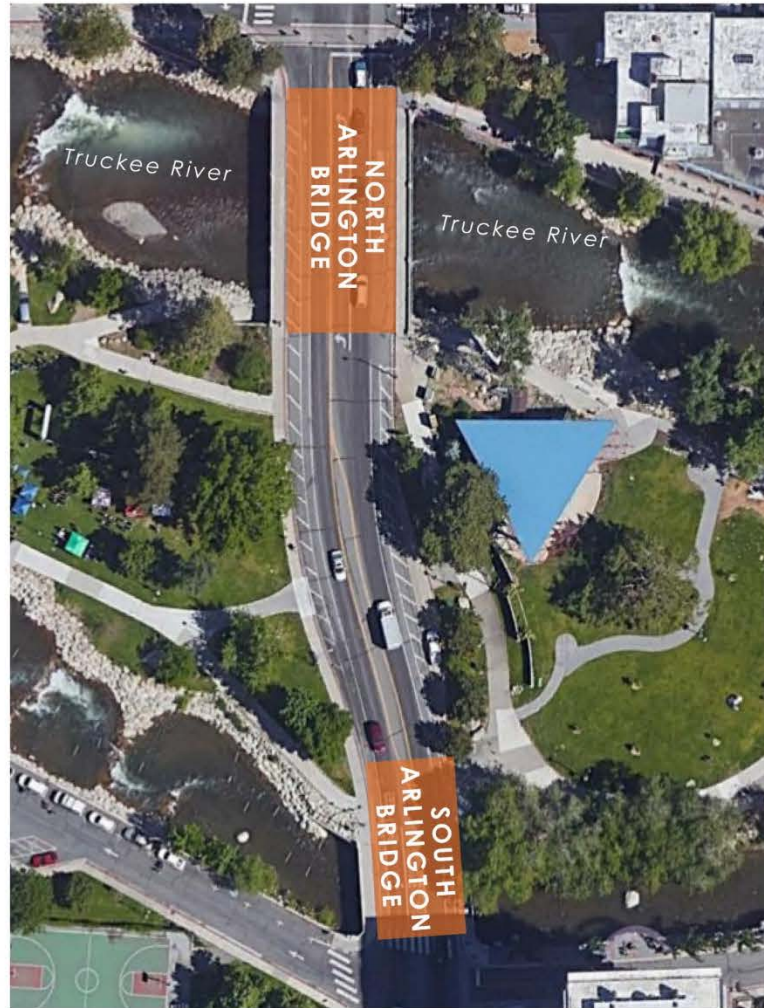


PEDESTRIAN ACCESS





# Existing Conditions Inventory: Arlington Avenue Bridges



SITE PLAN

ROCKERY FLOOD WALL

SOLID CONCRETE BARRIER RAIL

PEDESTRIAN ACCESS

RIP RAP

GIRDER

DOUBLE PIER BRIDGE



NORTHERN BRIDGE SECTION, LOOKING EAST



VIEW OF NORTHERN BRIDGE

ADJACENT PARK

ADJACENT RIVERWALK

SOLID CONCRETE BARRIER RAIL

CLEAR SPAN BRIDGE

PEDESTRIAN RAILING



SOUTHERN BRIDGE SECTION, LOOKING EAST



VIEW OF SOUTHERN BRIDGE



# Existing Conditions Inventory: Arlington Avenue Bridges



NORTHERN BRIDGE SECTION, LOOKING WEST



SOUTHERN BRIDGE SECTION, LOOKING WEST



VIEW OF KAYAK PARK



VIEW OF WILDLIFE



# Existing Conditions Inventory: Sierra Street Bridge

SMOOTH TEXTURED FLOOD WALL

SMOOTH TEXTURED PIERS

DOUBLE PIER BRIDGE

PEDESTRIAN LIGHTING

PEDESTRIAN ACCESS

DECORATIVE BARRIER RAIL



SITE PLAN



VIEW LOOKING WEST



VIEW LOOKING NORTH



LIGHTING



SIDEWALK



OPEN DECORATIVE CONCRETE RAILING





# Existing Conditions Inventory: Virginia Street Bridge

TEXTURED  
FLOOD  
WALL

DECK WIDENED  
FOR PEDESTRIAN  
USE

CLEAR SPAN  
BRIDGE

PEDESTRIAN  
LIGHTING

ART DECO  
BRIDGE  
ACCENT

VEHICULAR  
RATED  
INTERIOR  
GUARDRAIL

TRANSPARENT ART  
DECO INSPIRED  
PEDESTRIAN  
RAILING



SITE PLAN



VIEW OF BRIDGE



RAILING



LIGHTING

SALVAGED/  
REPLACED  
LIGHT FROM  
HISTORIC  
BRIDGE



BRIDGE  
ACCENT  
LIGHTING

RAILING  
ACCENT  
LIGHTING

PEDESTRIAN  
SCALED  
LIGHTING



DECORATIVE PAVING





# Existing Conditions Inventory: South Center Street Bridge

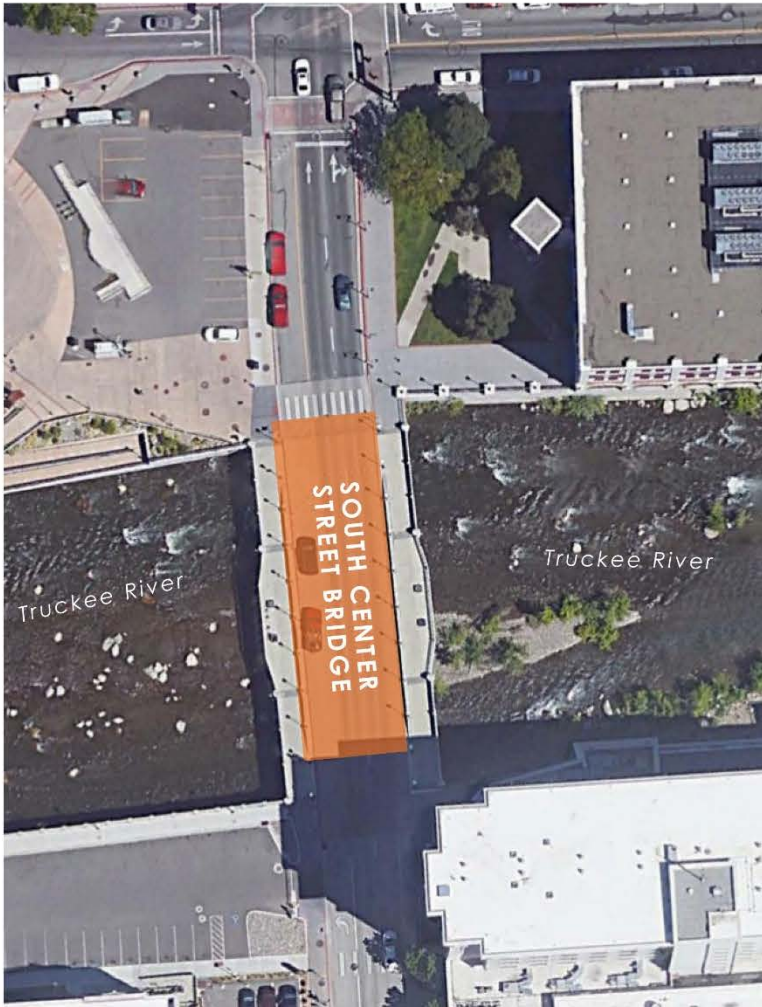
TRANSPARENT ART  
DECO INSPIRED  
DECORATIVE RAILING

VEHICULAR RATED  
DECORATIVE  
CONCRETE INTERIOR  
GUARDRAIL

SCORED  
PAVING

DECK WIDENED FOR  
PEDESTRIAN USE

PEDESTRIAN  
SCALE LIGHTS



SITE PLAN



VIEW LOOKING EAST



VIEW LOOKING WEST



LIGHTING



DECORATIVE RAILING AND GUARDRAIL



# Existing Conditions Inventory: Lake Street Bridge



SITE PLAN

RENO WELCOME SIGN

DECORATIVE RAILING

PEDESTRIAN ACCESS

DECORATIVE LIGHTING ON BRIDGE PILASTER



PERSPECTIVE LOOKING SOUTHEAST



LIGHTING



REMOVABLE METAL RAILING





# Existing Conditions Inventory: Riverwalk



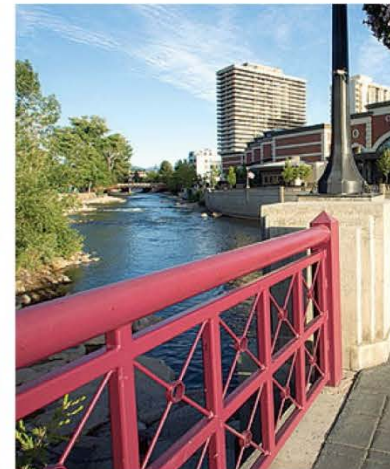
SITE PLAN



PERSPECTIVE



ARCHITECTURAL FEATURES



RAILING



PEDESTRIAN BRIDGES



# Existing Conditions Inventory: Artwork





# Site Inventory Map

The central image is an aerial photograph of the Wingfield Park area in Reno, NV. The Truckee River flows through the center, with Wingfield Park situated on its west bank. Key streets shown include West First Street, West Street Plaza, Arlington Ave, and Island Ave. The map is annotated with callout lines pointing to various features, each accompanied by a small inset photograph. On the left side, the callouts are: URBAN ART (pointing to a bridge over the river), KAYAK PARK (pointing to a rocky section of the river), BRIDGE PATHWAY (pointing to a stone bridge), PARK PATHWAYS (pointing to a paved path), SPORTS COURTS (pointing to tennis courts), and BARBARA BENNETT PARK (pointing to a basketball court). On the right side, the callouts are: NORTH FLOOD WALL (pointing to a concrete wall), AMPHITHEATER (pointing to an outdoor performance area), PEDESTRIAN ACCESS (pointing to a path), STREET TREES (pointing to a row of trees), and PEDESTRIAN BRIDGE (pointing to a small bridge over a stream). The map also includes labels for 'Truckee River', 'WINGFIELD PARK', and 'WEST STREET PLAZA'. A north arrow is located in the bottom right corner of the map area.

**URBAN ART**

**KAYAK PARK**

**BRIDGE PATHWAY**

**PARK PATHWAYS**

**SPORTS COURTS**

**BARBARA BENNETT PARK**

**WEST FIRST STREET**

**TRUCKEE RIVER**

**WINGFIELD PARK**

**ARLINGTON AVE**

**ISLAND AVE**

**WEST STREET PLAZA**

**NORTH FLOOD WALL**

**AMPHITHEATER**

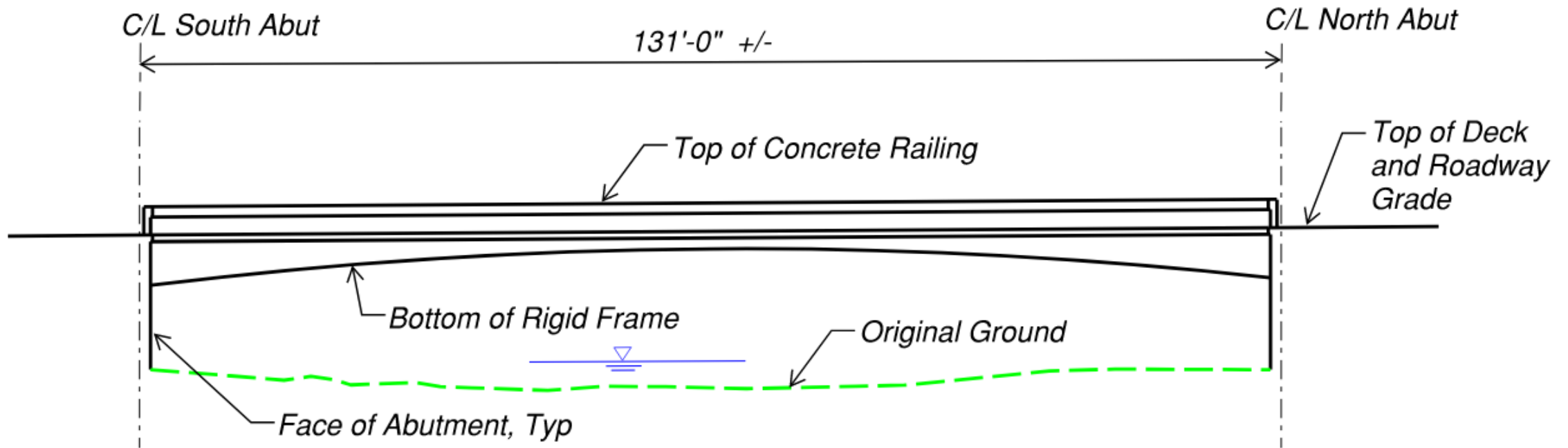
**PEDESTRIAN ACCESS**

**STREET TREES**

**PEDESTRIAN BRIDGE**



# Clear Span Bridge Concept



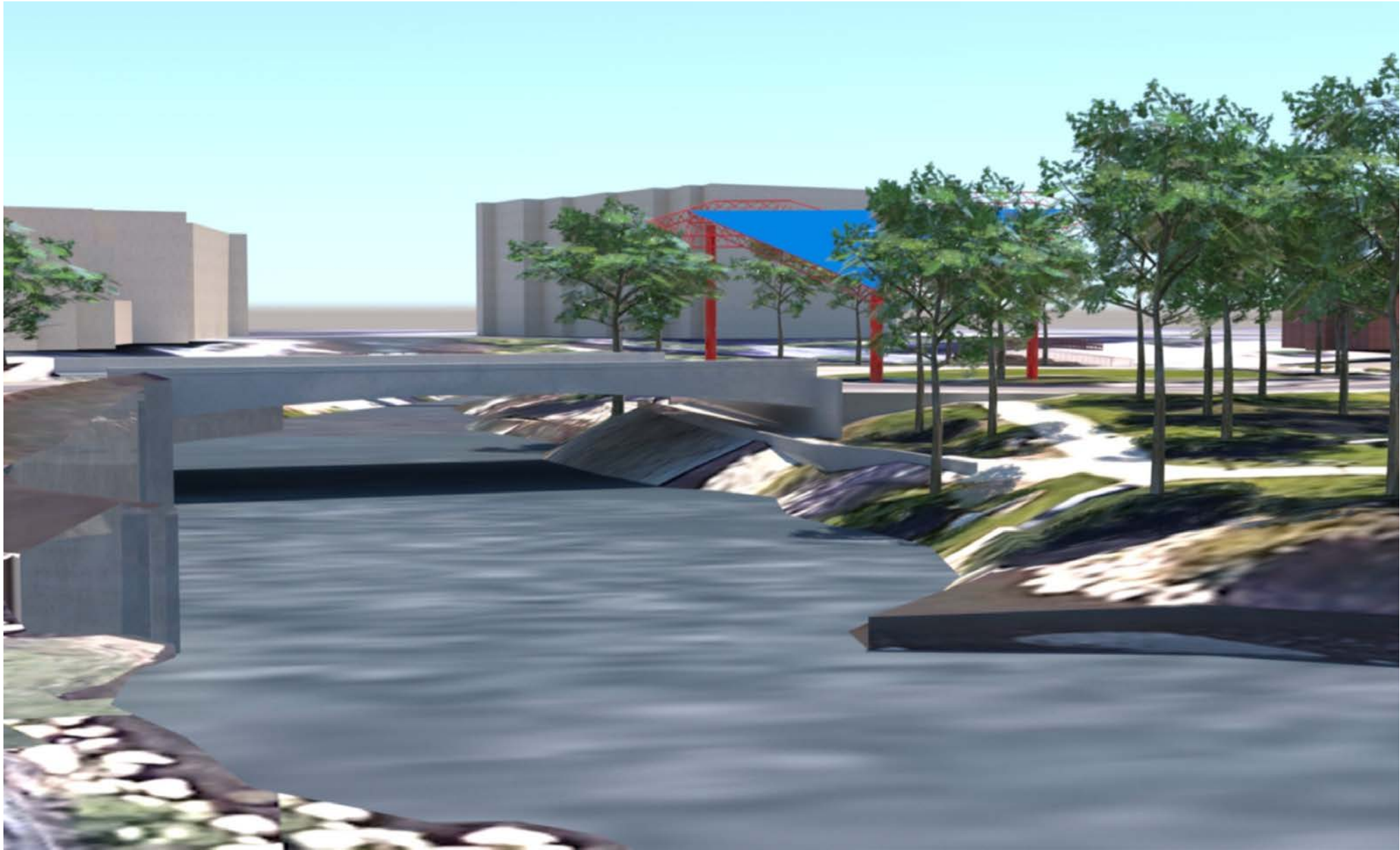
## Pros:

- No obstruction in the river improves flow capacity and boating access
- No pier/surface for tagging

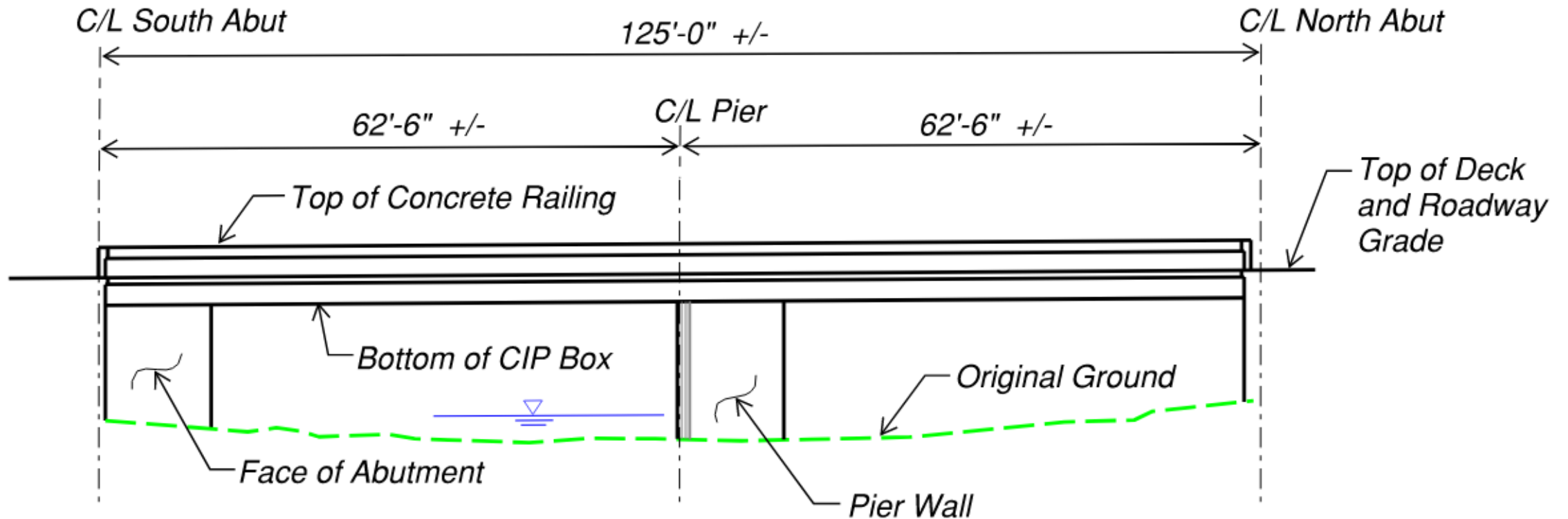
## Cons:

- Challenges in expanding the sidewalk at midspan for overlook/viewing area
- Depth of structure is deeper than single pier and provides less pedestrian overhead below bridge

# Clear Span Bridge Concept



# Single Pier CIP Concrete Box Bridge Concept



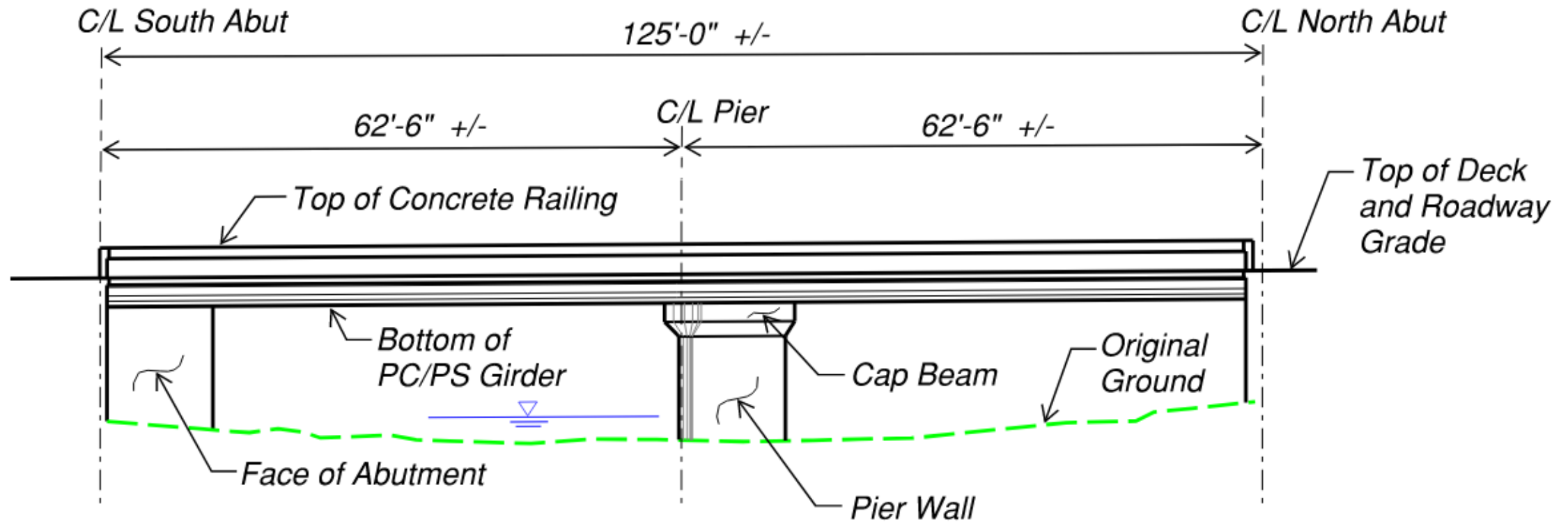
## Pros:

- Pier provides means to support widened overlook/viewing area mid-river
- Shallower bridge depth minimizes impact to roadway profile

## Cons:

- Obstruction in river for recreational boating users (although, one less pier than existing bridge).
- Pier wall may become a potential tagging surface

# Single Pier Precast Concrete Girders Bridge Concept



## Pros:

- Pier provides means to support widened overlook/viewing area mid-river
- Shallower bridge depth minimizes impact to roadway profile

## Cons:

- Obstruction in river for recreational boating users (although, one less pier than existing bridge).
- Pier wall may become a potential tagging surface



# Single Pier Bridge Concept





# Opportunities and Constraints Map



## LEGEND

- Pedestrian Access
- Bicycle Access
- Pedestrian footbridges provide ample pedestrian access from adjacent amenities
- Existing floodwall needs renovation, providing an opportunity to update the formliner pattern
- Reno Riverwalk termination, bringing foot traffic to Arlington Avenue
- Opportunity to replace existing pedestrian railing to match new bridge aesthetics
- Landscape may require regrading if street elevation is raised; maintain existing trees and add street trees
- Views of Truckee River
- 1** Park amenities include tennis courts, basketball courts, play equipment, and restrooms attracting visitors of all ages
- 2** Park amenities include large grass areas, tree lined pathways, bike and pedestrian access, and river recreation
- 3** Amphitheater hosts concerts in the park, making this location a premier summer destination
- 4** Whitewater park includes drop pools, smooth rocks, deep pools, with many access points, attracting many visitors in summer months
- 5** Existing stone steps are not accessible and poorly designed
- 6** Existing utility boxes are an eyesore, consider relocating
- 7** Street is closed for special events, between First/Island, maintain smooth pedestrian traffic flow across.



# Design Goals:

## Cohesive Design Language

1. Unify the north bridge and south bridge experience with a consistent form language, including the experience on the bridges and viewing the bridges
2. Establish a project theme to unify all the bridge and landscape elements

## Enhance Pedestrian Experience

1. Arlington Avenue to act as an urban plaza, using unified materials between sidewalk and street
2. Maintain vantage points of the river and surrounding landscape
3. Enhance pedestrian experience with shade trees, decorative lighting, decorative railing, paving, and sculptural/artistic features

## Contextual and Historical Relevance

1. Proposed structural elements will have relevance to the urban context
2. Project shall pay homage to Reno's history, while representing a new age of bridge development within the downtown core

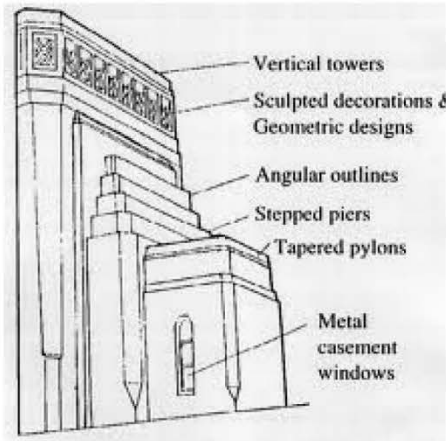
## Innovation and Sustainability

1. Low Impact Development (LID)- Street shall be repaved using permeable pavement, concrete pavers, permeable concrete
2. LED lighting
3. Drought resistant and native trees and plantings

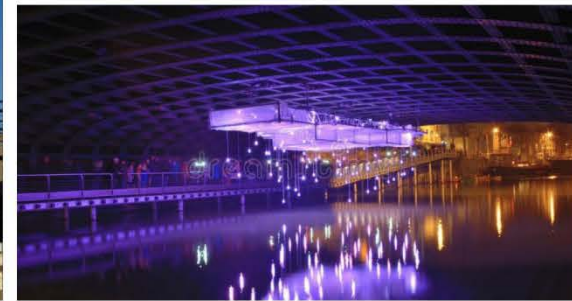
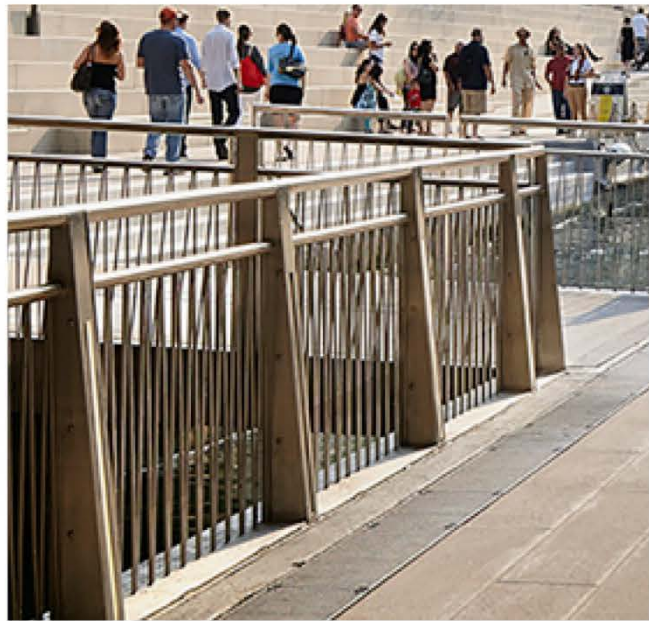
# Proposed Aesthetic Elements

- Modern Design Elements, A Melding of Old and New
- Pedestrian Scaled Lighting
- Bridge Accent Lighting
- Under Bridge Lighting
- Transparent, Traffic Rated Bridge Railing
- Maintain Pedestrian Accessibility
- Widen Bridge Deck
- Textured Abutment Walls
- Flood Walls
- Plaza Street

# Modern Design Elements: A Melding of Old and New



- Proposed decorative elements should incorporate modern design elements with a nod to the Art Deco historical context
- Decorative elements will focus on pedestrian lighting, railing design, under-bridge lighting, and sculptural elements
- Consider applying decorative form liner under bridges and on bridge pilaster and girders





# Pedestrian Scaled Lighting



COLUMN LIGHTS



STICK LIGHTS

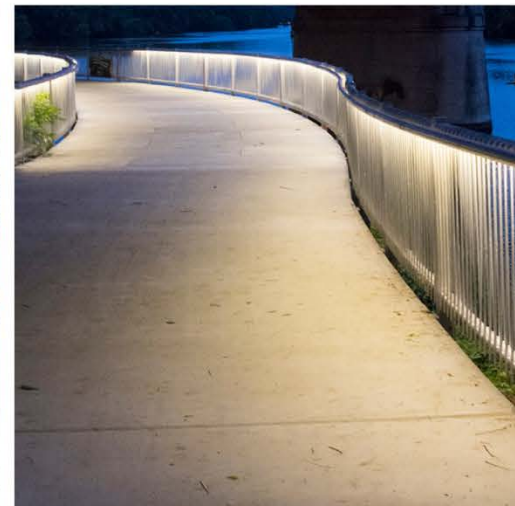
- Potential for modern pedestrian lighting concept
- Potential for stick lights fixed into bridge pilaster on north and south bridge
- Lighting elements to continue between bridges to create unity
- Incorporate lighting elements into railing
- Lighting along renovated north flood wall to match with existing art deco-style lights



LIGHTED BOLLARD



RAILING LIGHTING



# Bridge Accent Lighting



- Potential girder lighting to unify the north and south bridge
- Precast girders provide areas to conceal lights
- Potential bridge pilaster lighting
- Consider lighting protection from flood water, debris and vandalism.
- Consider impacts to aquatic species



GIRDER LIGHTING



PILASTER LIGHTING

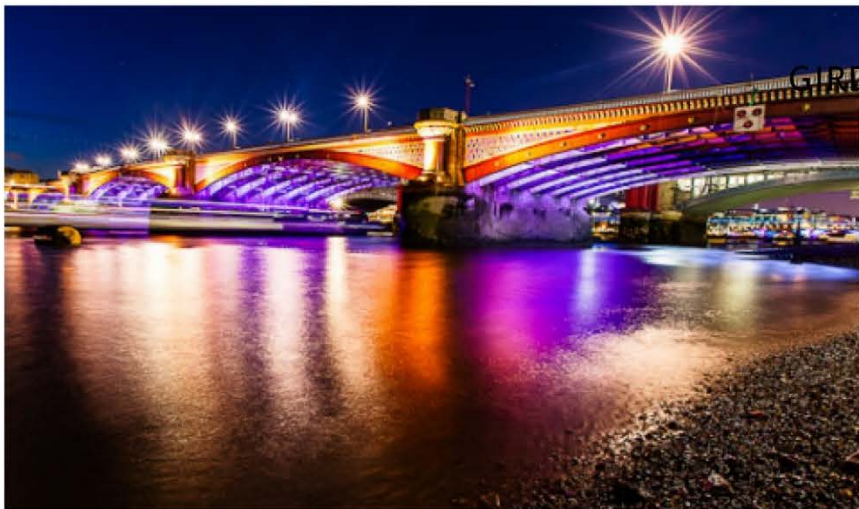


# Under Bridge Lighting

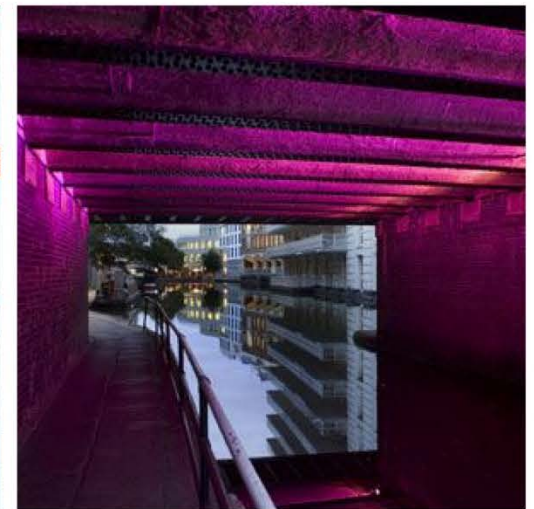


UNDER BRIDGE LIGHTING

- Potential under-bridge aesthetic lighting and under-bridge safety/pedestrian lighting
- Consider a lighting concept that can be applied to other bridges in downtown to create a “family” of unified bridges
- Consider aesthetic lighting experience for visitors viewing the bridges and visitors walking along the bridges
- Provide conduit for future bridge artistic lighting



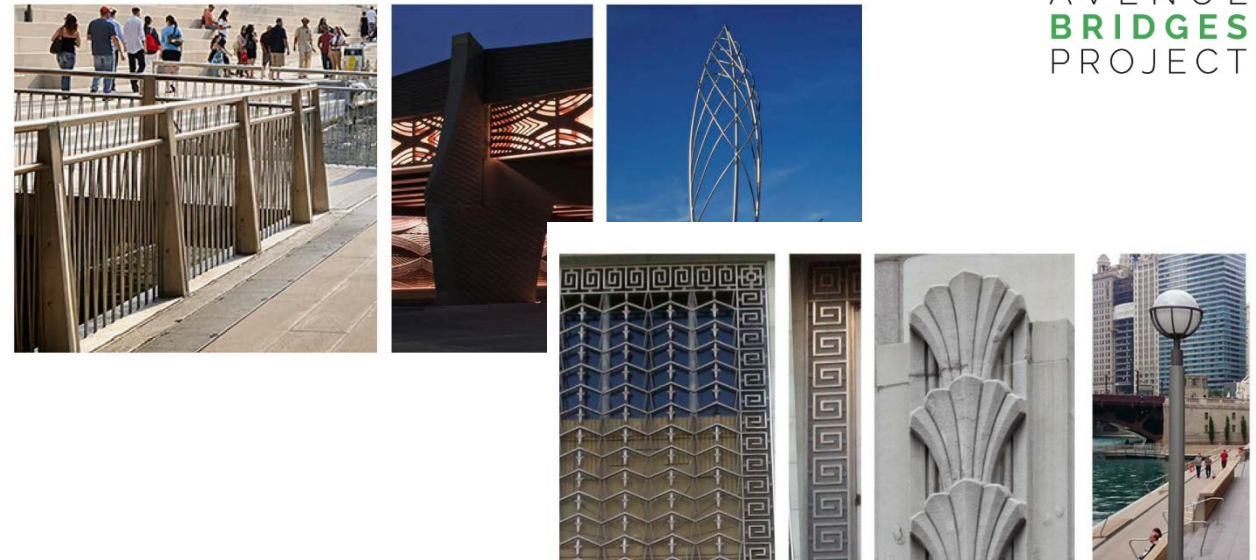
COLORED UNDER BRIDGE LIGHTING





# Questions on Theme and Lighting?

- ▶ Is the general theme ok?
  - ▶ Modern design elements with a nod to the Art Deco historical context



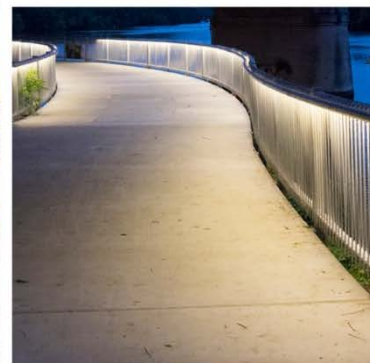
- ▶ Should lighting be included?
  - ▶ Pedestrian Scaled, Bridge Accent, Under Bridge



LIGHTED BOLLARD



RAILING LIGHTING



UNDER BRIDGE LIGHTING

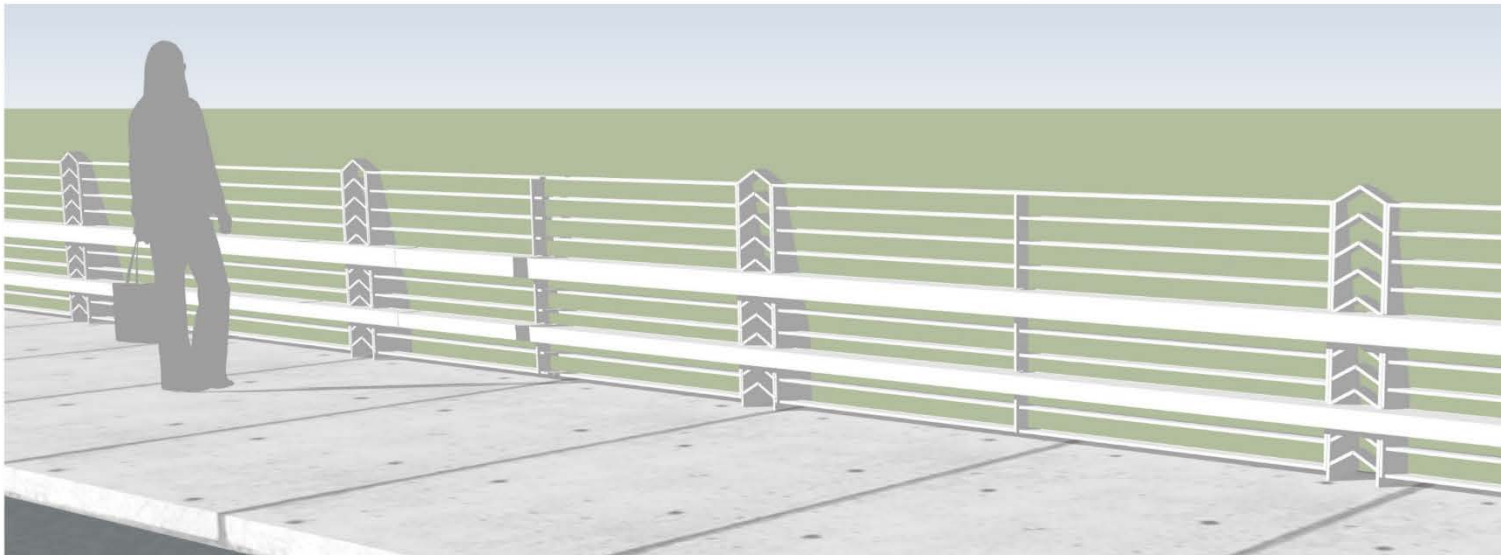


# Transparent, Traffic Rated Bridge Railing



OPEN DECORATIVE CONCRETE VEHICULAR GUARDRAIL

- Exterior railing should allow for viewing of the river
- Avoid double railing due to large number of special events
- Potential for decorative concrete railing and decorative steel railing hybrid
- Potential vehicular rated metal guardrail with custom artistic detailing
- Art deco concrete railing exists along northern flood wall, Sierra Street Bridge, and Center Street Bridge



TYPE TL-4 VEHICULAR BARRIER WITH PEDESTRIAN MODIFICATIONS



DECORATIVE CONCRETE BARRIER RAIL AND STEEL RAILING HYBRID



# Maintain Pedestrian Accessibility



CENTER STREET BRIDGE DOUBLE RAILING



STREET TREES



- Avoid double bridge railing due to large number of special events and need for pedestrian accessibility
- Maintain smooth pedestrian movement across street even if elevation needs to be raised
- Consider planting street trees along Arlington Avenue at 30 ft. O.C., if elevation needs to be raised
- Provide graffiti coating for maintenance/ easy removal

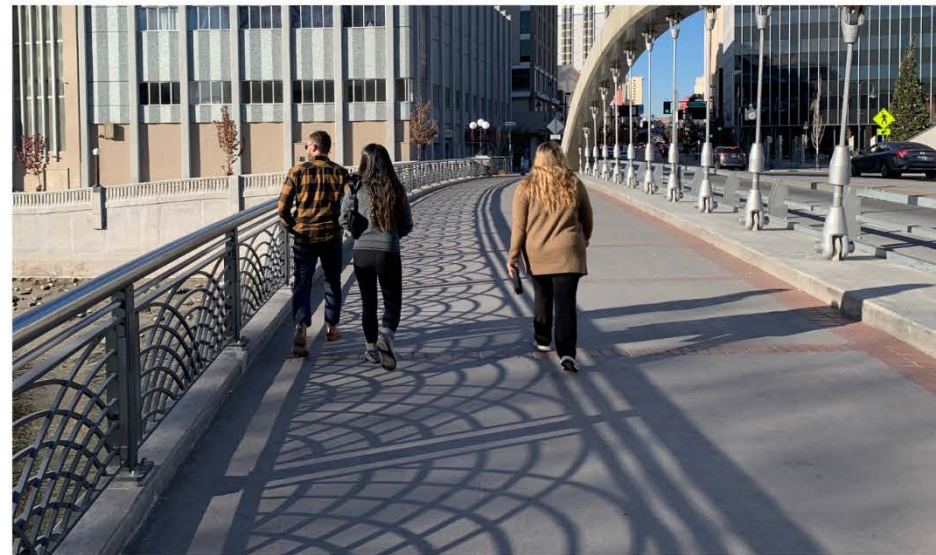


# Widen Bridge Deck

- Consider widening bridge deck to allow for greater pedestrian viewing (single pier only)
- Center Street Bridge and Virginia Street Bridge have widened segments



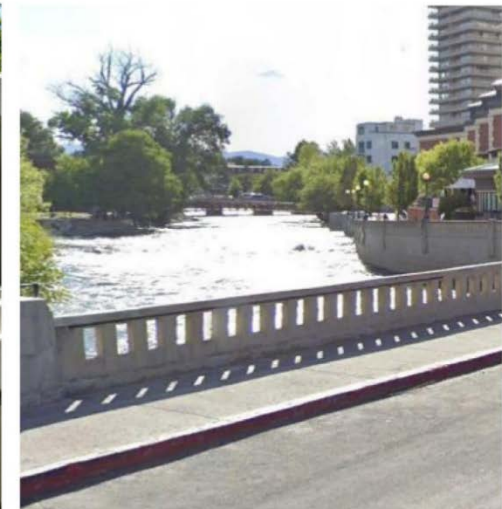
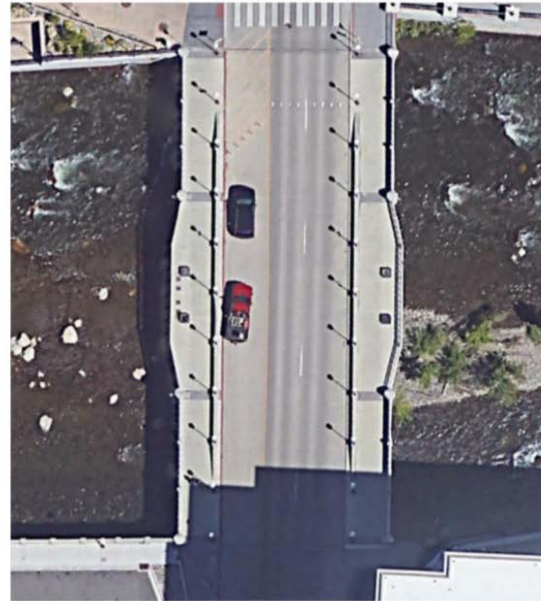
WIDENED BRIDGE SEGMENT





# Questions on Railing and Sidewalk Space?

- ▶ Smooth Pedestrian Movement
  - ▶ Don't include double railing?
  - ▶ Should railing be transparent?
  - ▶ Should the bridge deck be widened?



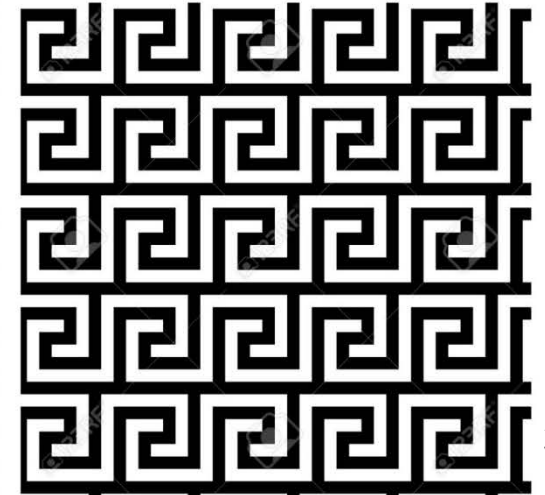
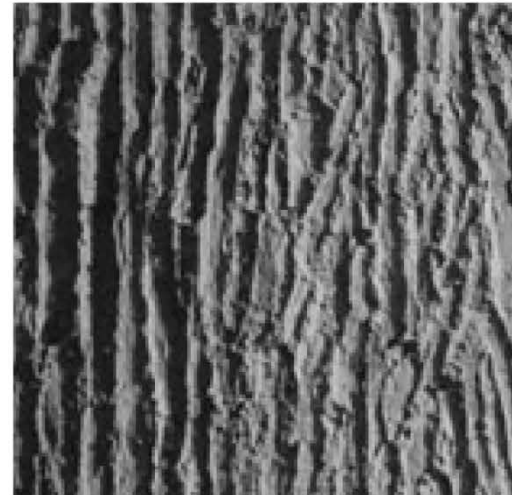
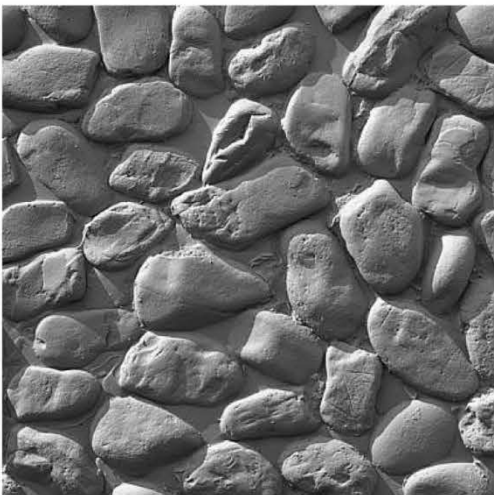


# Textured Abutment Walls

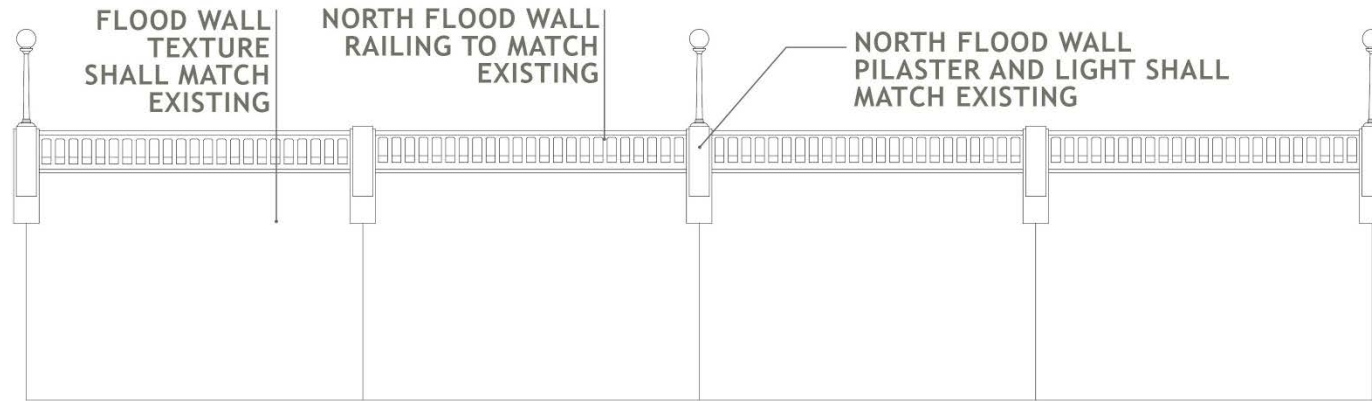


FORMLINER PATTERN FOR  
ABUTMENT WALL

- Consider abutment wall with a rich texture to enhance pedestrian and river user experience below the bridge
- Consider cobble texture used elsewhere



# Flood Walls



NORTH FLOOD WALL MOCK UP



EXISTING FLOOD WALL



FORMLINER PATTERN FOR FLOOD WALL

- Flood wall should have consistency between bridges
- Textures, horizontal reveals, open concrete railing, pilasters extended below, and lighting shall be consistent
- Flood wall along south side shall match with cobble wall along Riverwalk
- Consider a narrow planting at bases of walls to soften the height



# Plaza Street



STREET PLAZA



- Consider Arlington Avenue 'Plaza Street' concept to unify park areas if street between bridges needs to be replaced
- Consider permeable pavers on street and sidewalk to create seamless transition and provide stormwater infiltration
- Sidewalk paving with unique stamp or sandblast that follows theme



WATER WAVE CONCRETE PATTERNS



DECORATIVE PAVING

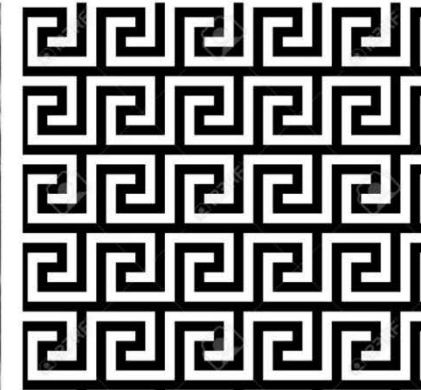
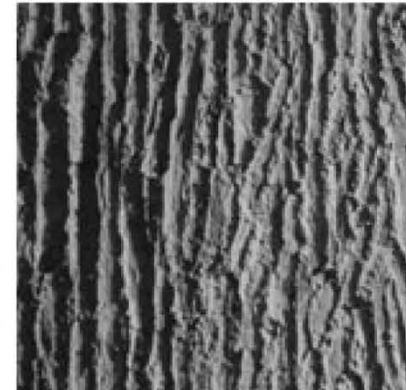
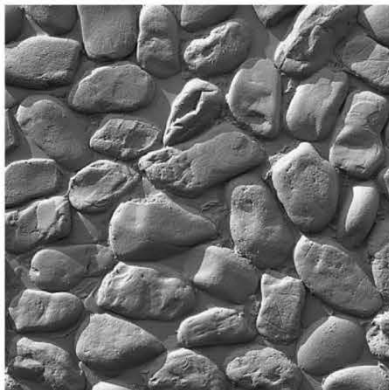


PERMEABLE PAVERS



# Questions on Surface Texture?

- ▶ Is the Plaza Street idea appealing?
- ▶ Should texture on abutment walls and flood walls be considered?



# Group Discussion and Consensus



- ▶ Theme and Lighting
- ▶ Railing and Sidewalk Space
- ▶ Surface Texture
- ▶ Additional Ideas

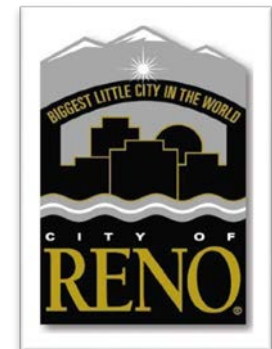


# Thank you for Participating!



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**JACOBS**



Meeting ID	Topic	Start Time	End Time	User Email	Duration (Minutes)	Participants
	81010211622 AAB SWG-3	12/15/2020 12:45	12/15/2020 15:16	jtortelli@rtcwashoe.com	151	30
Name (Original Name)	User Email	Join Time	Leave Time	Duration (Minutes)		
Judy Tortelli	jtortelli@rtcwashoe.com	12/15/2020 12:45	12/15/2020 15:16	151		
Sunshine Reporter Brandi Smith	brandiavsmith@gmail.com	12/15/2020 12:46	12/15/2020 15:16	151		
Toni Mollett Harsh		12/15/2020 12:49	12/15/2020 15:16	147		
Barb Santner		12/15/2020 12:49	12/15/2020 15:16	147		
Anne koloc-buja		12/15/2020 12:51	12/15/2020 15:16	145		
Brian Saeman	bsaeman@unr.edu	12/15/2020 12:55	12/15/2020 13:58	63		
Mike Cooper		12/15/2020 12:56	12/15/2020 15:16	140		
Ken Greene		12/15/2020 12:57	12/15/2020 15:16	139		
Kayla Dowty - CTWCD (Tri Sage Consulting)	kschlichting@trisage.com	12/15/2020 12:57	12/15/2020 15:02	126		
Kerrie Koski		12/15/2020 12:57	12/15/2020 14:58	121		
Jonathan Simpson		12/15/2020 12:58	12/15/2020 15:09	132		
Dale.D.Wegner		12/15/2020 12:58	12/15/2020 15:16	138		
kelly		12/15/2020 12:59	12/15/2020 15:16	138		
Lauren Ball	lball@rtcwashoe.com	12/15/2020 12:59	12/15/2020 14:41	103		
John L'Etoile (iPad)		12/15/2020 12:59	12/15/2020 15:16	138		
Jack		12/15/2020 12:59	12/15/2020 14:06	68		
Megan Berner		12/15/2020 12:59	12/15/2020 15:07	129		
Matt Brezina		12/15/2020 12:59	12/15/2020 15:03	125		
Michon		12/15/2020 13:00	12/15/2020 15:15	136		
	17757868853	12/15/2020 13:00	12/15/2020 15:15	135		
Laurie Leonard		12/15/2020 13:01	12/15/2020 15:15	135		
Gregory Erny		12/15/2020 13:01	12/15/2020 14:23	83		
Claudia		12/15/2020 13:01	12/15/2020 15:16	135		
Matt Negrete/Jacobs		12/15/2020 13:01	12/15/2020 15:15	135		
Doug Maloy	dmaloy@rtcwashoe.com	12/15/2020 13:02	12/15/2020 14:59	118		
Travis Truhill (City of Reno)		12/15/2020 13:02	12/15/2020 14:58	117		
Fr.Chuck		12/15/2020 13:07	12/15/2020 14:40	94		
Downtown Reno Partnership	online@downtownreno.org	12/15/2020 13:08	12/15/2020 14:23	76		
Theresa Jones	tajones59@charter.net	12/15/2020 13:12	12/15/2020 15:11	120		
Brian Stewart	bstewart@rtcwashoe.com	12/15/2020 13:58	12/15/2020 15:02	65		



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REGIONAL TRANSPORTATION COMMISSION  
ARLINGTON AVENUE BRIDGES PROJECT  
STAKEHOLDER WORKING GROUP

---o0o---

RTC Stakeholder Working Group-3 Meeting  
Tuesday, December 15, 2020  
Reno, Nevada

24 Reported by: Brandi Ann Vianney Smith  
25 Job Number: 698917

1 ---o0o---

2 RENO, NEVADA, TUESDAY, DECEMBER 15, 2020, 1:00 P.M.

3 ---o0o---

4

5 MS. TORTELLI: Well, welcome everybody. I think  
6 we can go ahead and get started.

7 I would like to ask everybody to mute your mics.  
8 It looks like, Michon, if you could mute your mic maybe?

9 And I know just asked everybody to mute their  
10 mics, but can you see my screen?

11 MR. COOPER: Yes, we can see your screen.

12 MS. TORTELLI: All right. Thank you. We'll go  
13 ahead and get started.

14 I wanted to welcome everybody. I am Judy Tortelli,  
15 Project Manager for the RTC. I am here today to talk about  
16 aesthetic themes for the Arlington Avenue Bridges Project.

17 I have two folks that are going to help me with  
18 the presentation today. One is Barb Santer, Senior  
19 Landscape Architect with Stantec, and Mike Cooper,  
20 Structural Engineer with Jacobs.

21 I also want to let everybody know that I do have a  
22 court reporter on the call to help capture meeting notes.

23 Today we will run through a presentation and go  
24 over what the team has put together regarding bridge  
25 aesthetics.



1 Barb will be covering the majority of the  
2 presentation, and Mike will talk about the bridge concepts  
3 that were carried forward from our last Stakeholder Working  
4 Group meeting.

5 I would like to ask that as we go through the  
6 presentation, you keep your mics mute, and please make a  
7 note of any questions or comments you may have.

8 I do have several breaking spots within the  
9 presentation to allow time for questions and discussions.

10 I am not going to go through a full attendance  
11 because we have everybody that's logged on.

12 Brandi, just so you know, I will send you a report  
13 from Zoom on who logged on.

14 But I did want -- if my team could just introduce  
15 themselves, please, so people know who is going to be  
16 presenting.

17 MS. SANTER: I'm Barb Santer, Landscape Architect  
18 with Stantec Consulting here in Reno.

19 MR. COOPER: I'm Mike Cooper, Bridge Engineer with  
20 Jacobs.

21 MS. TORTELLI: All right. Thank you, guys.

22 So the purpose of today's meeting is to discuss  
23 aesthetics for the project.

24 When we started this study, several aesthetic  
25 themes were identified. They included the Downtown Street

1 Streetscape Mater Plan, mimicking the Virginia Street  
2 Bridge, applying a family of bridges theme to establish a  
3 standard for future bridge replacements, or creating a new  
4 theme specific to the Wingfield Park area.

5 We got great feedback on the tail end of our  
6 second Stakeholder Working Group meeting regarding  
7 aesthetics.

8 It was noted that the Downtown Streetscape Master  
9 Plan didn't really include the bridges, and the Group felt  
10 following this would box us in and limit our flexibility.

11 At Stakeholder Working Group-2, there was a lot of  
12 discussion about the family of bridges theme. It would be  
13 nice to create somewhat of a standard since several of the  
14 Downtown bridges are going to be replaced in the near  
15 future.

16 Based of that discussion, our theme at this point  
17 is to have modern design elements with a nod to the art deco  
18 historical context. This theme will help shape how we move  
19 forward.

20 We have already determined which bridge concepts  
21 should be carried forward. Now it's time to discuss their  
22 appearance.

23 The aesthetic elements we present to you today are  
24 pretty high level. We have some flexibility here to play  
25 with the appearance of the bridges and don't want to box



1 ourselves in.

2 We need your help to guide us on what should be  
3 carried forward for additional analysis as we move forward  
4 into the phase of the project, which is NEPA and design.

5 Here is an agenda. We will review the site  
6 history specific to the Arlington Avenue Bridges, existing  
7 conditions of the Downtown bridges, and look at how to  
8 recommended bridge concepts affect aesthetic elements.

9 The team has done a great job defining our design  
10 goals, and we have an overview map that lays out some of the  
11 opportunities and constraints.

12 There are eight aesthetic elements that we are  
13 proposing, and we will look at them in three categories.

14 The first will be overall theme and various  
15 lighting potentials.

16 Next, we will look at opportunities for railing  
17 and widening the sidewalk space.

18 We will wrap up by looking at some surface texture  
19 options.

20 There is a lot of material to look at, so I wanted  
21 to break it up and allow time for discussion in each  
22 category prior to moving onto the next.

23 I may be jumping in during the questions portion  
24 if discussions are getting off track or taking up too much  
25 time.

1 I want to ensure the group has the opportunity to  
2 see everything that proposed. So we may table some ideas  
3 and continue the discussion toward the end of meeting.

4 So here is a location map of Downtown bridges in  
5 Reno. Going from west to east, you have here the Booth  
6 Street Bridge, and then the Keystone Avenue Bridge. Moving  
7 further on down the river, is the Arlington Avenue Bridges,  
8 then the Sierra Street Bridge, the more-recently replaced  
9 Virginia Street Bridge is here, then the Center Street  
10 Bridge, and the Lake Street Bridge.

11 The Arlington, Keystone, and Sierra bridges are  
12 identified for replacement in the RTC 2040 Regional  
13 Transportation Plan in the years 2222 to 2226.

14 The Lake Street Bridge is slated for replacement  
15 in 2027 to 2040 timeframe.

16 I would like to note that the Lake Street Bridge  
17 replacement has some major issues from a flooding  
18 perspective.

19 Our focus here at the RTC is on transportation and  
20 maintaining our roadway system for all users and not  
21 necessarily performing flood control.

22 This is why the Lake Street Bridge falls within  
23 the later years. We did, however, put a significant  
24 Band-Aid on the Lake Street Bridge just last year.

25 So with four bridge replacements on the horizon,



1 the family-of-bridges theme looks more appealing and  
2 achievable.

3 Now, I am going to turn it over to Barb so we can  
4 jump into the fun stuff.

5 MS. SANTER: Thank you, Judy.

6 MS. TORTELLI: Um-hmm.

7 MS. SANTER: So we started our study of the  
8 Arlington Street bridge replacements by kind of looking at  
9 some bigger picture items, and the first thing was the  
10 history of site.

11 So we looked at history of what is now called  
12 Wingfield Park, which is where the bridges are located. And  
13 here are just some points about the history.

14 In 1909, Louis Hinckley purchased and developed  
15 the land into a relaxing, resort-like destination know as  
16 Belle Isle.

17 The river was partially dammed and a foot bridge  
18 was added in 1912.

19 In 1916, the owners of the park filed for  
20 bankruptcy, and the space was acquired by the Reno National  
21 Bank.

22 In 1920, the influential banker, George Wingfield,  
23 bought the property and donated it to the City. The island  
24 was later named George Wingfield Park.

25 In 1912, the north bridge structure was

1 constructed. It was originally called the Chestnut Street  
2 Bridge, and it is Nevada's oldest T-beam bridge.

3 In 1928, a flood caused substantial damage to the  
4 park with repairs paid for by George Wingfield.

5 Then from 1925 to the 1980s, the Reno Municipal  
6 Christmas tree was standing at Wingfield Park.

7 You can see a variety of photos on this page.  
8 Here on the upper left is a photo of the original Belle Isle  
9 Amusement Company site plan showing some various amusement  
10 rides and such, and it looks like this was actually before  
11 many other improvements were made in the park.

12 The lower left, you can see a photo showing some  
13 of the amusement features in the park that were brightly  
14 lit. It looks like a carousel and some other features.

15 The lower center photo is another one from 1911  
16 showing a women viewing over the river into, it looks like,  
17 the island with a ferris wheel, and you can see some other  
18 amusement rides.

19 Then on the lower-right side, you can see a  
20 postcard featuring this really beautiful photo of the park  
21 with some large cottonwood trees and et cetera.

22 So to summarize, this was a really important park  
23 in Downtown Reno for over a hundred years.

24 Another aspect of this area was just kind of an  
25 overall design inspiration, and looking to the famous book



1 The City of Trembling Leaves, written by Walter Van Tilberg  
2 Clark, the book talks a lot about the history and  
3 development of Reno.

4 Reno was established as a small town on the edge  
5 of the mountain range with beautiful trees and wildlife, and  
6 the book illustrates the natural beauty of Reno landscape  
7 and highlights Wingfield Park as a leisurely oasis within  
8 the city.

9 Of course since then, urbanization and development  
10 has changed the landscape of the city within the last  
11 century, but we kind of question how can the proposed design  
12 reconnect to the history of this area and celebrate the  
13 natural beauty of the Reno landscape.

14 A couple notable aspects about, of course, the  
15 title of the book referring to the city of trembling leaves  
16 was the fact that here at this location in the Great Basin  
17 with the Truckee River, I think travelers recognized that  
18 there were significant cottonwood trees and kind of a  
19 beautiful setting here that was kind of rare within the  
20 landscape of the Great Basin.

21 So next, we wanted to look at the existing  
22 conditions of the Downtown bridges. The reason for doing  
23 this was to kind of look at the context of the architectural  
24 design of all the bridges, and then, of course, focusing in  
25 on the Arlington Street bridges, but recognizing that, as

1 Judy mentioned earlier, a number of bridges still require  
2 replacement in the Downtown.

3           So we were intrigued with the idea of looking at  
4 what features we might be able to design for the Arlington  
5 Street Bridge that would tie into other Downtown bridges,  
6 and also, perhaps, be able to be used for future bridge  
7 replacements.

8           So going from west to east, the first one we  
9 looked at was the Booth Street bridge.

10           So the upper photos, you can see a photo looking  
11 at the bridge, looking south, and you can see it has a  
12 smooth, solid concrete railing. It's got some accent  
13 lighting on each end of the bridge, kind of a globe light.

14           It's a single-pier type of bridge design, has a  
15 smooth abutment wall. There is some riprap along the sides  
16 for some slope reinforcement. And, again, on the right, you  
17 can see looking from the actual Booth Street looking to the  
18 river, you can see the textured, solid concrete wall with a  
19 sidewalk.

20           The lower middle, you can see a more closeup view  
21 of the globe lights, another view of the sidewalk with the  
22 solid concrete railing, and pedestrian access that takes the  
23 traveler into Idlewild Park.

24           Next, we're not looking in detail at the Keystone  
25 Bridge because that one is kind of different style than what



1 we're looking at replacing here for Arlington.

2           So we're moving instead to the Arlington Avenue  
3 bridges, which is the next set of bridges to the east.

4           So, as you know, there are two bridges along this  
5 area because they connect the island over the two branches  
6 of the Truckee River from north to south. So the north  
7 bridge is the larger bridge over the larger portion of the  
8 river.

9           You can see the first photo in the upper middle,  
10 again, this bridge also has a solid concrete barrier rail.  
11 This is looking east. The floodwalls nearby in this portion  
12 have natural rock texture close to the bridge, and then it  
13 transitions to solid concrete. Then there's a sidewalk just  
14 adjacent to that railing.

15           On the upper-right side, you can see a view of the  
16 northern bridge, and this is actually as one would be  
17 standing on the path on the west side of the bridge in the  
18 park looking up at bridge.

19           So you can see this existing bridge has two piers  
20 in the water. You can see the girders on the underside.  
21 The riprap in this case is actually from the more-recent  
22 construction of the kayak park, and, as I mentioned, in the  
23 photo standing on the path looking up at the bridge. So  
24 it's notable because this bridge has pedestrian access  
25 underneath to connect both sides of island.

1           On the bottom of this photo is a photo of the  
2 southern bridge section. So this is connecting the smaller  
3 branch of the Truckee River.

4           So this one is looking east. Again, it's got the  
5 solid concrete panels that are smooth. You can see,  
6 actually, a view of the Riverwalk Project, which is very  
7 distinctive, built right in the very early 90s, and that has  
8 a stone cobble-textured wall, and kind of the dark  
9 cherry-colored railings and distinctive features.

10           The right side is a photo looking at the southern  
11 bridge. I believe that's looking from the south looking at  
12 it. There are some pilasters on the end.

13           One notable aspect is, looking at some history of  
14 the north side of the Arlington Avenue Bridge, there used to  
15 be lights, apparently, on both ends of the bridge, but they  
16 were removed after a significant flood, and they were not  
17 replaced.

18           Next are some more photos looking from the bridge.  
19 On the upper side, you can see, looking west, it's a really  
20 nice view of the kayak park and the boulders that were added  
21 actually create a really nice texture, in addition to  
22 creating the really nice pools and riffles in that portion  
23 of the river. That has now kind of become Reno's largest  
24 swimming hole pretty much.

25           On the upper right side is a southern bridge



1 looking west, and that, you can see the kayak gates for  
2 kayak competitions.

3           On the lower left is another view of the kayak  
4 park. Really pretty with lots of trees, and then a little  
5 shot -- there's actually some wildlife down there. There  
6 are ducks, and I know there is a green heron that actually  
7 lives on the east side of the island that I just last week  
8 again.

9           Moving to the east looking at the Sierra Street  
10 Bridge, this one, if you look at the upper-middle photo,  
11 that bridge has also got a concrete railing with openings,  
12 and it's got lights on these, kind of, art-deco-looking  
13 pilasters, and it's a two-pier -- a double-pier bridge.

14           View looking north on the street, you can see down  
15 the sidewalk, the sidewalk's directly adjacent to the  
16 barrier railing -- or excuse me the bridge railing, and you  
17 can see the pedestrian-scaled lights. There is a closeup of  
18 the lighting in the lower middle, and a closeup of the  
19 existing railing, which, by the way, just a note about that  
20 railing, I do not believe that railing would meet current  
21 accessibility requirements, which requires a four-inch-wide  
22 maximum opening, and I think this is a litter wider than  
23 that.

24           On the lower right side, you can see an  
25 interesting shot that has a view of the bridge railing, and

1 then looking to the east, it's a view of the floodwall along  
2 the Truckee River lane. The same exact railing design was  
3 actually extended along the floodwall for quite a long  
4 distance, and it has the pilasters that extend down the wall  
5 and kind of a globe light that is used intermittently pretty  
6 much throughout to floodwall from Virginia Street down to  
7 Arlington.

8           Next is the Virginia Street Bridge, which, of  
9 course, was replaced and opened just a few years ago. That  
10 bridge is a clear-span type. It doesn't have a pier in the  
11 water. I believe it's called a tied arch.

12           So it's got the large concrete arches. It has,  
13 kind of, an art deco feature at the end of the concrete  
14 arch, which has a layered concrete appearance.

15           One notable feature about this bridge, which you  
16 can see in the plan view on the left, is that the deck was  
17 intentionally widened with that bowed-out appearance. The  
18 reason for that was to provide a little more generous space  
19 for pedestrians to look at the water and gather, as this  
20 whole Downtown area is really pretty heavily used for  
21 special events and other features.

22           On the upper-right side, you can see that the  
23 outside railing has a distinctive arch design with a powder  
24 coated dark gray, and then the top of the railing is  
25 actually stainless steel, and it has a light built in that

1 casts down and creates a really nice shadow. That was  
2 designed to kind of tie into the arch design of the bridge.

3 Because of the tied-arch design, an interior  
4 railing was required to protect vehicles from driving off  
5 the edge of bridge. So this bridge has a double-railing  
6 system.

7 In lower middle, you can see one of end pilasters  
8 from the Virginia Street Bridge, and this actually features  
9 a cluster of lights that was salvaged from the historic  
10 Virginia Street Bridge, and it was placed on the ends of the  
11 bridge on all four corners.

12 The lower, kind of the middle one, you can see  
13 some night lighting, and there is some really nice lighting  
14 of the railing and the arch itself.

15 Then the lower right, you can see the sidewalk  
16 paving, which is kind of a concrete with brick header design  
17 that has some tie to the existing Downtown Streetscape  
18 sidewalk concrete.

19 Although, I will say those standards have now been  
20 revised, and the brick header and stamped concrete are no  
21 longer part of the Downtown Streetscape Standards; it's just  
22 dark gray concrete with scoring.

23 Okay. Looking at next bridge to the east is the  
24 Center Street Bridge, and Mike Cooper and I worked on this  
25 design in --



1 Mike, was it about 1996? I want to say.

2 MR. COOPER: Yes.

3 MS. SANTER: Yes. So this bridge was replaced  
4 around 1996. This really was another design that kind of  
5 drew upon the nearby context of the Post Office -- the  
6 Downtown Post Office, which was art deco at the time. The  
7 Mapes Hotel was actually still standing, then the Riverside  
8 Hotel.

9 So going through some architectural concepts and  
10 process, it was decided to kind of nod to the art deco on  
11 this one, which nods pretty heavily in this case.

12 So the upper middle, you can see it's got a double  
13 railing, a transparent art deco inspired railing. Then on  
14 the -- that's on the very outside of the bridge where it's  
15 widened, and the inside railing is a concrete railing,  
16 vehicular rated, and Judy's pointing to that.

17 So because of the fact that the exterior railing  
18 in the center, the bridge deck was widened. We had to have  
19 this double railing system.

20 So again, looking at the plan view on the left,  
21 you can see how the bridge deck was widened, and, once  
22 again, that was based on input from stakeholders and the  
23 idea that it would be really a nice place to look at the  
24 river and have a really beautiful transparent railing in  
25 that location.

1           The lighting was this globe light, again, kind of  
2 art-deco inspired, and the lower right shows a close up of  
3 the overlook the bridge widened area with the interior  
4 vehicular-rated railing that has the lights on it, and then  
5 the super-transparent's, sparkly aluminum railing on the  
6 outside.

7           Those features with the chevron were really drawn  
8 and inspired from the Downtown Post Office that has aluminum  
9 grill work with those features.

10           Moving east to the Lake Street Bridge, which is  
11 one of the ones Judy mentioned would be, perhaps, replaced  
12 on the 2027 timeframe, this bridge on Lake Street has a  
13 removable railing, and that's due to the flooding issues  
14 here. So this one is a more utilitarian railing instead of  
15 being very much -- very elaborate.

16           You can see the old Reno arch on this photo in the  
17 upper right that it was placed near the auto museum. These  
18 decorative lights on this bridge look extremely similar to  
19 the Sierra Street Bridge, actually, placed on pilasters, so  
20 I believe there are four of those total. There is a closeup  
21 of that light on the pilaster in the lower middle.

22           Then you can see that railing, which is removable  
23 because of flooding issues, on the lower right.

24           Another important feature of the Downtown that  
25 kind of threads a couple of these areas together, and it's

1 not a bridge, but it's the Reno Riverwalk.

2           The first segment, which on the right there where  
3 the cursor is between Virginia and Sierra was replaced in  
4 the very early 90s -- I want to say like, maybe, 1991 -- and  
5 there is a photo of it lower left looking at it from the  
6 Virginia Street Bridge.

7           It's super distinctive with dark cherry, metal  
8 powder-coated railing and those are pergolas, which have the  
9 caps made of the dark cherry railing.

10           It's got the chain and bollard with stainless  
11 steel, and the extremely distinctive is also the floodwall.  
12 It's made with a natural stone veneer, and it's got a  
13 concrete cap that is natural granite. It has gotten damaged  
14 during some Downtown floods, but it is a really  
15 pretty-looking floodwall.

16           In the middle second one on the bottom, you can  
17 see another closeup showing some of the super-distinctive  
18 features. Those were custom pedestrian-scaled lights, and a  
19 custom several railing designs. These little arbors that  
20 have benches below them, and there's a closeup on the photo  
21 next to that, that color is a little more, maybe,  
22 exaggerated than how it really appears, but that is what a  
23 close up of what the railing looks like in one spot.

24           Then on the lower right, there is a pedestrian  
25 bridge extending from the Riverwalk over to Wingfield Park



1 that has also been colorized with the dark cherry.

2 I might make a note that the Riverwalk dark cherry  
3 color, that cherry color used to be on all of the Reno  
4 Downtown traffic signal standards and lights. It was  
5 painted to black about, maybe, ten years ago or so , because  
6 I think the thought was not to use this dark cherry  
7 elsewhere in Downtown anymore.

8 So then, just to point out, the second phase of  
9 the Riverwalk went from Sierra Street to Arlington Avenue  
10 just extending that whole treatment.

11 There is also some very customized granite that  
12 several colors, very elaborate granite that was put on all  
13 of the pedestrian surfaces, but during the 1997 flood, I  
14 think it was, it was so heavily damaged between Arlington  
15 and Sierra that that was replaced with the stamped, ashlar  
16 concrete. So it's still in place between Sierra and  
17 Virginia, but not in the left side any more.

18 Another aspect of the Downtown in this area is it  
19 is a very popular area for Reno's Public Art Program. There  
20 are a number of really significant pieces placed down here,  
21 and it's really kind of the heart of the arts and culture  
22 district, I would say.

23 The upper two left pieces are at Bicentennial  
24 Park -- actually, the one in the middle may have been  
25 removed at this point, because that's intended for pieces to

1 change periodically.

2 But the upper-right piece is at the Nevada Museum  
3 of Art. Of course, the Believe sculpture is at First  
4 Virginia -- or City Plaza, and then we have murals  
5 throughout the Downtown.

6 So it's definitely been a popular district,  
7 including Wingfield Park, to place a lot of art of all  
8 different types.

9 So now, honing in on our location again, this site  
10 inventory map is a plan view showing the island and, again,  
11 the location of the two bridges on Arlington Avenue, and  
12 just has some photos to kind of remind us that the context  
13 is as we're looking at this area.

14 So we have, of course, Wingfield Park on the  
15 island, the West Street Plaza on the upper right. It's West  
16 First Street on the top . The lower left is Barbara Bennett  
17 Park on Island Avenue, which has basketball and tennis  
18 courts.

19 There are some photos throughout the margins here.  
20 On the upper left, you've got a photo of sculpture at  
21 Bicentennial Park, photos of the kayak park, the pathway  
22 under the bridge that I was referring to earlier, sport  
23 courts at the park, and a mural.

24 On the upper right, you can see photos of the  
25 floodwall on the north side, the Wingfield Park

1 amphitheater -- super popular for summer performances --  
2 pedestrian access ways, a lot of street trees, and then,  
3 again the pedestrian bridges.

4           There are three pedestrian bridges close by here.  
5 There's one on the southern branch of the river, and then  
6 one connecting Wingfield Park to West Street Plaza above  
7 that, and another one on west side connecting Wingfield,  
8 basically, to Bicentennial Park.

9           So with that, Mike Cooper is going to talk about  
10 our bridge concepts.

11           MR. COOPER: So just a brief summary of what came  
12 out of the Technical Advisory Committee meetings and the  
13 Stakeholder Working Group meetings we've had already, kind  
14 of a review of the three concepts that came out of that  
15 work.

16           The first one is this rigid-frame structure. It  
17 kind of has an arched shape to it. It's similar -- I was  
18 noticing in the picture Barb has of the southern bridge on  
19 Arlington -- it has the same kind of lines to it, only a  
20 much smaller span on that bridge.

21           So the advantage of the rigid frame, it eliminates  
22 the obstructions in the north branch of the Truckee.  
23 Currently, the bridge there has two supports in the river.

24           So those would be removed entirely with this  
25 option. A side benefit of that is there would be no pier



1 surfaces for any kind of tagging activities that might go on  
2 with access under the bridge.

3           Some of the challenges with this structure, with  
4 the thin section at mid-span, it's going to be a little bit  
5 more challenging to expand the sidewalk there beyond the  
6 standard width, similar to what Barb was talking about at  
7 Virginia Street and Center Street. It's not impossible, but  
8 there are some structural challenges with that just due to  
9 the section of the bridge there.

10           Then the other thing to point out near the  
11 supports faces of the abutments, the section of the bridge  
12 is deeper, and with the path that goes underneath the south  
13 end of this bridge, there may be -- it might impact headroom  
14 there for pedestrian access underneath the bridge.

15           So this is -- we've just begun to generate some 3D  
16 renderings. So these are kind of raw at this state. These  
17 will get cleaned up as we move ahead.

18           But this is intended to give you kind of a picture  
19 of what that area might look like with supports removed from  
20 the channel. The clear span of this rigid frame over the  
21 river, the pathway within Wingfield Park passing underneath  
22 the bridge there on the south end of bridge.

23           So the other two concepts that came out of the  
24 previous work, they are both two-span bridges, single pier  
25 concepts.

1           This first one would be a cast-in-place concrete  
2 box girder bridge. The bottom side of this structure,  
3 looking at it from underneath, you would see, basically, a  
4 solid concrete slab overhead rather than discrete girders on  
5 the bridge.

6           The pier wall that you see there in the middle of  
7 the elevation view looks wider than it is because what  
8 you're really seeing is sort of the skewed image of the face  
9 of pier; that darker are on the one edge of that really  
10 depicts more of the width of what that pier wall would look  
11 like.

12           So the -- with a center pier, it provides a means  
13 to support a wider deck more readily than the rigid frame  
14 might. It also provides a shallower overall structure depth  
15 that will help to mitigate any profile changes on the  
16 roadway above on Arlington, and also provide more headroom,  
17 potentially, underneath the bridge for the pathway in  
18 Wingfield Park.

19           In terms of drawbacks of this alternative, there  
20 would still be one obstruction in the river, although it's  
21 one less obstruction that exists there today, and depending  
22 on access and water levels and what not, the pier wall  
23 itself may become an attractive nuisance as a tagging  
24 surface.

25           So the other similar concept would be precast

1 concrete girders. A couple of differences that you would  
2 note in this structure, you'll see on the center, it appears  
3 that there is a widened cap at the top of that pier wall,  
4 and that's intended to have a place to set the concrete  
5 girders as they are placed during construction. So it's a  
6 little bit different shape of the pier.

7           Then from underneath the bridge, you wouldn't see  
8 a solid concrete surface that you would see with the  
9 cast-in-place box; you'd see the individual lines of the  
10 precast girders.

11           Similar pros and cons for this -- the precast  
12 concrete girders and the cast-in-place box.

13           So, again, the beginnings of our 3D renderings.  
14 This shows you what the pier wall might look like in the  
15 channel.

16           This particular image is of the precast concrete  
17 girder bridge. So you can kind of see the end of the cap  
18 element that would be on top of that pier that would  
19 initially support those precast girders.

20           These two single pier concepts would also provide  
21 means to get the pathway underneath the bridge at the south  
22 end.

23           Back to Barb.

24           MS. SANTER: Okay. So the next level of study  
25 that we prepared was looking at, kind of, a classic element



1 of analysis that we call "opportunities and constraints." .

2 So looking at those options, looking at some of  
3 the opportunities and constraints, I am just going to read  
4 from the legend and notes on the right side here so you can  
5 follow along.

6 The dashed, orange line is pedestrian access. So  
7 you can see that there's a lot of pedestrian access here  
8 along West First Street, along the floodwall, which is kind  
9 of where the number 7 is. Throughout Wingfield Park,  
10 there's a loop on both sides of Arlington Avenue.

11 Most significantly, there is pedestrian access  
12 below the north bridge on the south side at the abutment  
13 wall.

14 Of course, there is access going east along Island  
15 Avenue, and also which is along the Riverwalk and Truckee  
16 River Lane to West Street Plaza and Virginia Street.

17 There is also bicycle access on the Arlington  
18 Avenue bridges, and then there are those three pedestrian  
19 foot bridges in pink that are noted in the pink dash line.

20 Going to further down the legend, we notice that  
21 had some of the existing flood walls, which are shown by  
22 that black diagonal dash, they are looking a little  
23 deteriorated. It might be that that needs to be replaced in  
24 the future, and, maybe, there's an opportunity to look at  
25 the form liner patterns for those or the treatment so that

1 there is some consistency with other floodwalls.

2 That circle represents just the terminus of the  
3 Reno Riverwalk, and that kind of launches people into  
4 Wingfield Park from the south side.

5 On the black dash line, there is a portion of  
6 railing on the northeast side of the Arlington Avenue Bridge  
7 along the floodwall that doesn't match any other railing.  
8 It's like a pipe railing, and it looks like it might have  
9 been a later retrofit.

10 There is another small piece of it on the  
11 southeast side of the Arlington Avenue Bridge, so those  
12 could possibly be replaced and be consistent with other  
13 treatments.

14 In the green area, the lighter green with the  
15 trees is indicating an area that, should the street have to  
16 be elevated to make the bridges join with the adjacent  
17 streets, it might require some regrading in those light  
18 green areas.

19 It doesn't look like right now it's going to be  
20 substantial or significant, but if that should need to  
21 happen, the thought is that it would be best to maintain  
22 some really smooth pedestrian access across the street  
23 because of the number of pedestrians that are down here  
24 during special events.

25 And if it does have to be regraded, although we

1 would like to save existing trees, we could also add some  
2 street trees in those locations.

3           The yellow areas just indicate views of the  
4 Truckee River.

5           Number 1 points out some of the park amenities,  
6 such as tennis courts, basketball, play equipment,  
7 restrooms, attracting visitors of all ages.

8           Number 2 shows some large grass areas, tree-lined  
9 pathways, bike and pedestrian access, and river recreation.

10           Number 3 points out the actual amphitheater, which  
11 hosts numbers of concerts in the summer, making this  
12 location a really premier summer destination, especially  
13 during the Art Town month of July.

14           Number 4 points out the Whitewater Park features,  
15 including some drop pools, smooth rocks, deep pools, many,  
16 many access points and attracting many visitors in the  
17 summer months.

18           A few fine points that were pointed by folks in  
19 the Parks Department right where number 5 is, there are some  
20 existing stone steps that are -- I don't know that it's  
21 really referring to that spot right there, but there are  
22 some existing tone steps that are a little bit difficult to  
23 access creating some difficulties. Some existing utility  
24 boxes in that location that are kind of right behind the  
25 sidewalk and a bit of an eyesore that, if the project should



1 impact them, it would be, maybe, nice to relocate those  
2 elements.

3           Number 7 is looking at some -- excuse me for a  
4 second. I need to stretch my screen.

5           Just pointing out -- this is really important. So  
6 7 is pointing out the fact that there are quite a few  
7 special events where the entire street is closed off between  
8 Island Avenue and West First Street, and it makes this whole  
9 thing kind of one big special event area.

10           Some notable events of that are particularly the  
11 Kayak Festival, where there are actually booths set up on  
12 Arlington Avenue, we have bike races -- it used to be a bike  
13 race, there were, obviously, no events this year, but last  
14 year we had a couple of bike races, and, of course, many Art  
15 Town events.

16           So I guess I kind of see this as almost like a  
17 park that has a street going through it, and a really  
18 important area for maintaining pedestrian access.

19           So some of the goals we were looking at -- now  
20 that we have studied all these features, some of our design  
21 goals that we thought were important are shown on this  
22 slide.

23           One of them is to use cohesive design language  
24 with, number 1, unifying both the north bridge and south  
25 bridge experience using consistent form, and also looking at

1 the experience on the bridges in viewing the bridges. So  
2 just using some consistent design elements.

3 Number 2, establish a project theme to unify all  
4 the bridge and landscape elements.

5 Then the second big goal was enhancing the  
6 pedestrian experience.

7 Under that, number 1, Arlington Avenue could act  
8 as kind of an urban plaza using unified materials that have  
9 a similar look between the sidewalk and Street.

10 The second one would be to maintain vantage points  
11 of the river and the surrounding landscape.

12 The third, enhance the pedestrian experience with  
13 more shade trees, decorative lighting, decorative railings,  
14 paving, and some sculptural and artistic features.

15 The third goal is regarding the contextual and  
16 historical relevance.

17 Under that, number 1 is we propose that the  
18 structural elements should have some relevance to the  
19 existing urban context.

20 Number 2, the project should pay homage to Reno's  
21 history; however, we feel the need to represent a new age of  
22 bridge development within the Downtown core, rather than  
23 completely replicating history.

24 The fourth goal would be to look at innovation and  
25 sustainability. Given the location here that is right on

1 the Truckee River, we thought that it could be important to  
2 look at what's called "low impact development strategies,"  
3 which is ways that storm water can be infiltrated at the  
4 source. One way to do that would be, possibly, if the  
5 street needs to be repaved, we could use permeable pavements  
6 or concrete pavers so that the storm water infiltrates right  
7 there and not have to have an extensive storm water system  
8 of directing water directly into the river via pipes.

9 Another topic under that would be just to use --  
10 what is pretty common now -- LED lighting, which is  
11 extremely low-energy-requiring lights.

12 So now we get into the final section of today's  
13 meeting, which is the proposed aesthetic elements.

14 So as we were studying all these bridges, old and  
15 new, and the context of the Downtown, we thought that it  
16 would be important to look at several elements of what could  
17 be designed -- how the bridge could be designed and present  
18 to you some ideas that we think are important and get your  
19 feedback.

20 So here is a list of all the elements. Again, the  
21 first one is we're thinking it should be a modern design,  
22 but it should be a melding of old and new.

23 We want to look at pedestrian-scaled lighting to  
24 the bridges. Possibly bridge accent lighting. Under-bridge  
25 lighting, especially given the fact that this north



1 Arlington Avenue Bridge has a sidewalk underneath that we  
2 would want to perpetuate.

3           Look at transparent, traffic-rated bridge railings  
4 instead of the current one that is solid where you can't  
5 really see the river from your car.

6           Maintain pedestrian accessibility on the bridge as  
7 well as on the street, should it have to be elevated.

8           Possibly widen the bridge deck like was done on  
9 Virginia and Center Street.

10           Look at the option of textured bridge abutment  
11 walls. Look at some options for floodwalls.

12           Then the idea that street could perform as sort of  
13 a plaza street just with some minor modification.

14           So now we're going to look at these things  
15 individually.

16           So the first topic, modern design elements, a  
17 melding of old and new. We're proposing the decorative  
18 elements should incorporate modern design elements, but have  
19 a nod to the art deco historical context.

20           The decorative elements would focus on pedestrian  
21 lighting, railing design, under-bridge lighting, and  
22 sculptural elements.

23           We would consider applying the decorative form  
24 liner under the bridges and on the bridge pilasters and  
25 girders.

1           Here are just some reference images some of the  
2 art deco context, which is in the upper slides, showing what  
3 some of the ways of designing the concrete are on the upper  
4 left that are kind of typical from that timeframe.

5           Some of the grill work that you see in the second  
6 photo is from the Post Office with the chevron and the Greek  
7 key design is that veritable strip, and then some other  
8 flairs of art deco.

9           The light on the upper right is actually a new  
10 light that was just put in the new downtown Chicago  
11 Riverfront project that is also kind of a more modern take  
12 on art deco.

13           Lower left is a kind of a modern railing, but it  
14 has a fair of some of the geometric patterns.

15           Then the bridges -- other photos on the lower  
16 slides just show some other elements that could be  
17 considered for this bridge in terms of lighting the exterior  
18 girders or some special elements like shown in the third  
19 slide on the bottom.

20           Then on the far right side, some elements of  
21 lighting that it could be applied.

22           The second topic that we would like to propose is  
23 that we should have pedestrian-scaled lighting on the  
24 bridges.

25           And, again, as I mentioned, there were lights on

1 the north bridge on each end. I don't have a photo of what  
2 they looked like, but there were lights that got damaged  
3 during a flood. But we do propose adding lights in this  
4 replaced-bridge concept.

5           The lights on the top are more art-deco flair.  
6 Again, that one in the upper left is a modern, brand new  
7 light that was just used on the Chicago Riverwalk project  
8 that is kind of a modern art deco.

9           Second one on the right is the actual globe light  
10 that was used on the Center Street Bridge, which, by the  
11 way, after it was put on Center Street, that light has now  
12 been used on Truckee River Avenue all the way from  
13 Bicentennial Park to Booth Street. That same light has been  
14 used, and it's also on the floodwall on the north side.

15           Another option would be these stick-type of lights  
16 that are just kind of way-finding little orbs. Those could  
17 be fixed into bridge pilaster on the north and south bridge.

18           The lighting elements could continue between  
19 bridges to create unity.

20           There's also the opportunity to incorporate  
21 lighting elements into the railings either like you see on  
22 the lower right where it's in the railing and it casts down  
23 similar to Virginia Street Bridge on the far right side.

24           Or it could be, possibly, put into the vertical  
25 bridge posts like the second photo from the left, which



1 actually, I believe, that railing is also from the newly  
2 redone Chicago Riverfront project.

3           Lighting along the renovated north floodwall,  
4 maybe, should match with the existing art-deco-style lights.  
5 And, again, that's the kind of lighting that's already there  
6 between Sierra and Arlington. It's got the globe type of  
7 art-deco-styled lights.

8           But lighting really brings so much life. It think  
9 it would definitely would be a positive addition.

10           So another aspect of lighting could be some bridge  
11 accent lighting.

12           There is a potential for some girder lighting, and  
13 the girder would be this horizontal element you see in the  
14 upper left to unify both the north and south bridge, the  
15 upper left and the lower left are both lighted girders, and  
16 that's a metal panel -- that's a jet-cut metal panel on the  
17 front that has a pattern that could basically be designed to  
18 whatever we want it to be, and then it's backlit.

19           Precast girders could provide areas to conceal  
20 lights under the bridge.

21           We have the potential for some other bridge  
22 pilaster lighting, and a couple of the things that have to  
23 be considered would be protecting lighting from vandalism  
24 and flood water and debris, given the location and  
25 possibility for flooding.

1           And then, also, we need to consider with any  
2 lighting under the bridge, or even on the bridge for that  
3 matter, impacts to aquatic species, given that there are a  
4 number of aquatic species in the river.

5           Under-bridge lighting is another aspect that we  
6 think could be really powerful for this bridge. There is a  
7 potential for under-bridge aesthetic lighting, and it could  
8 be kind of a combined under-bridge safety light and  
9 pedestrian lighting.

10           So for example, the photo on the lower right shows  
11 kind of a soft lighting, and you can see in this case, the  
12 individual girders give some kind of really interesting  
13 light effects.

14           Same with the all three of the bottom photos,  
15 actually, those are a girder-style bridge, which is one of  
16 our styles that Mike talked about, and that can have some  
17 pretty neat effects for lighting on the underside.

18           We might consider a lighting concept that could be  
19 applied to other bridges in the Downtown, and use the  
20 lighting to create the family of unified bridges. Lighting  
21 could be applied to other bridges, even though they don't  
22 look all exactly the same.

23           One thing we want to consider is the aesthetic --  
24 oh, maybe go back there really quick, Judy, if you don't  
25 mind.

1           Just see aesthetic lighting experience for  
2   visiters viewing the bridges, and also visiters walking  
3   along the bridges. Then we would, maybe, provide conduit  
4   for future bridge artistic lighting.

5           So given what we've shown you on the theme that  
6   we're suggesting and lighting, we wanted to open this up for  
7   some feedback to ask you questions.

8           So I guess the first thing is the general theme  
9   that we're proposing okay? And that is modern art design  
10  elements with a nod to the art deco historical context.

11          Then the second one, should lighting be included?  
12  Pedestrian-scaled lighting, bridge accent lighting, or  
13  under-bridge lighting.

14          So I guess we can open it up for your feedback and  
15  comments if you take yourself off of mute.

16          MS. BUJA: I would like to make one comment.

17          MS. SANTER: Sure.

18          MS. BUJA: Having worked in design for a long  
19  time, I love the idea of juxtaposing something very, very  
20  modern against something much more traditional and art deco.

21          If the bridges were really beautifully modern and  
22  the lighting emphasized the art deco or late, late Victorian  
23  theme, you would get that wonderful juxtaposition of the  
24  eras, and it would be interesting to look at.

25          My only question about lighting underneath the



1 bridges is how vulnerable is that lighting to vandalism?

2 And I will mute me now.

3 MS. KOSKI: This is Kerrie, Kerrie Koski at the  
4 City of Reno. I am the City Engineer. I concur with your  
5 comments about the lighting emphasizing the art deco  
6 elements.

7 I think the question about how vulnerable is the  
8 lighting, I guess the answer is: Depends.

9 As the department that has to maintain these  
10 structures, we would definitely want to take into  
11 consideration.

12 I truly believe that the lighting under the bridge  
13 could be such that it wouldn't -- it would be somewhat easy  
14 to maintain, but not yet difficult to vandalize. So I do  
15 agree with that.

16 I think that one of the things that we see -- we  
17 hear at the City of Reno is we often get requests for  
18 changeable lighting; different colors for different events.  
19 We often get those questions -- or requests.

20 So that would be something that I would like to  
21 throw out to the group.

22 With the respect to the on-the-bridge lighting,  
23 one theme that I saw --

24 And, by the way, Barb, you did a fantastic job  
25 going through the elements of this bridge and the area.

1 MS. SANTER: Thank you.

2 MS. KOSKI: Very good job. I think that -- the  
3 one thing I saw was the globe lights on to pilasters is kind  
4 of a continuous theme, and I liked what you said about using  
5 the lighting as kind of the concept to carry along the  
6 family of bridges. I think we're on to something there. I  
7 really do.

8 I did want to say one other thing about the bridge  
9 structure itself. Full disclosure, I have not been in  
10 support of the center pier, and I coming to the fact that I  
11 think the center pier is, maybe, my preferred now because I  
12 think it's going to give -- it going to break up the  
13 elements in the water.

14 I just want through this out to people. Right now  
15 under the Virginia Street bridge, it's all open under there.  
16 It's beautiful. It's vast. But it doesn't -- this bridge  
17 needs character of some sort.

18 I think that with the Whitewater Park, I think  
19 that would help break that up as well.

20 So the other thing I was thinking with the center  
21 pier is, to me, one of the major -- one of major things I  
22 think we should be looking at is the wider sidewalks, I  
23 think that's coming up next.

24 But I just wanted to throw that out there that I  
25 think that that center pier, we could also get wider

1 sidewalks, and I think that's the experience that people are  
2 looking at. They want to hang out there over the river and  
3 through the woods; right?

4 But, anyway, I really think that the wider  
5 sidewalks would be -- we'd get some real benefits out of  
6 that with that bridge structure.

7 Then I will be quiet for a while.

8 MR. STETTINSKI: This Alex from the Downtown Reno  
9 Partnership. I also want to thank Barb for a really cool  
10 presentation.

11 MS. SANTER: Thank you.

12 MR. STETTINSKI: And I agree with Ann and Kerrie's  
13 comments a hundred percent.

14 I think wider sidewalks are key. It's really  
15 important for the flow of our visitors walking around that  
16 neighborhood.

17 I love the under-bridge lighting. Also agree with  
18 the colors that Kerrie mentioned. I think it would be  
19 really cool to have some flexibility in the coloring of the  
20 bridges. And even though the bridges may look a little  
21 different, I think the colors could kind of unite the look  
22 at night really beautifully.

23 I also like the idea -- I'm looking at the current  
24 picture that is up on the screen, it's says "under-bridge  
25 lighting" to the very lower right, and I would assume that



1 the lighting also effects the path walk for visitors, for  
2 pedestrian.

3 That would be really, really important to me that  
4 this is really well lit, because there is a lot of stuff  
5 going on in Downtown to begin with.

6 I think the lighting will make have people feel  
7 safer walking along at night. Strolling along. And I think  
8 it's really beautiful to have the sidewalks lit anyway.

9 And, yeah, that's -- I also like Ann's comment  
10 about that combination of old and new to combine the history  
11 of the bridges with a more modern overlay.

12 I think this is going to be just stunning. It's  
13 going to be beautiful.

14 So that's my comment.

15 MS. SANTER: Thank you.

16 MR. L'ETOILE: Hi, Barb. This is John L'Etoile,  
17 Department of Plans and Architecture with the Department of  
18 Transportation.

19 I also agree, the theme of the architecture is  
20 appropriate. We have art deco in the area, but then, maybe,  
21 reinventing some of that art deco into a very contemporary  
22 appeal and affect, I think that's going to resonate  
23 throughout this section of Downtown very well with just  
24 these features complementing each other. So I really like  
25 that direction as well.

1           On the lighting, I just want to be a little  
2 cautious on the use of lighting to create a wow factor that  
3 really is not -- it becomes more of a decoration, rather  
4 than enhancing the beauty of the bridge structure itself for  
5 certain structural elements of the bridge itself.

6           But the color and the lighting up of the rhythm of  
7 the, maybe, the bridge structure or the girders and the  
8 under-lighting, I think that would be beautiful along the  
9 river.

10           And more of a subdued approach that -- I think  
11 Reno maybe has in this context of a little more history, in  
12 that context, we want to just be careful of lighting really  
13 does, again, become a feature that is tying this entire area  
14 together well, and not being too much loud -- too loud on  
15 it's own, but being special and being part of this area of  
16 having the bridge, like you said, connecting this park.  
17 That's a fabulous area, and the opportunities there are a  
18 lot of fun.

19           So that would be my only caution with lighting.  
20 Personally, I like lighting that lights up the beauty of the  
21 structure or the natural features of the area, but not  
22 becoming the decorative element in itself.

23           And the centerpiece -- I've always felt this about  
24 Reno -- is the river for Downtown Reno. It's just this  
25 amazingly beautiful life that continues through and connects

1 and has history wrapped into it, and the indigenous people  
2 there that used it and still do and enjoy that connectivity.

3 In light of that, I actually am in preference of  
4 the single-span bridge type. The reason for that is I  
5 think, again, it just allows you to see that river from so  
6 many more vantage points than you might otherwise with a  
7 large center piece there supporting the center of the  
8 bridge.

9 Mike brought up the challenges with that and the  
10 widened sidewalks. I do agree the widened sidewalks are  
11 super important. We want that pedestrian connectivity there  
12 and circulation.

13 But just thinking of the beauty of the water and  
14 the movement of the river and the wildlife in the river, and  
15 letting that really take center stage underneath the bridge  
16 and not the structure of bridge impeding with that center  
17 piece. Just my personal opinion on that.

18 Great presentation. Thanks.

19 MS. SANTER: Thank you.

20 FATHER DURANTE: I am going to jump in. This is  
21 Father Chuck from St. Thomas Cathedral. Sorry I signed a  
22 few minutes late, and I will have to probably leave before  
23 we finish.

24 I very much appreciate the presentation also and  
25 the wonderful thought put into it. I love the art deco and

1 the connections that we're placing.

2 I, too, am inclined to removing the pier. When I  
3 saw the drawing without that center piece, it just really  
4 caught my eye, and it created such a different feel for that  
5 river going through.

6 One thing I would say, a lot of times when the  
7 big, broader sidewalks are needed, that's when Arlington  
8 Street is closed anyway. So people aren't really on the  
9 sidewalks that much.

10 The only place might be following when there are  
11 concerts over there at the amphitheater, then sometimes that  
12 street's not closed. But I see the practical piece of  
13 making that a little broader. Something to just keep in  
14 mind, often the street's closed.

15 The only other point I'd make -- and I didn't  
16 catch this and may have just not heard it -- is the height  
17 of the walls along there.

18 Right now, I am waiting for the news -- I am sure  
19 it's happened -- that the bridge walls, as you're walking  
20 along the sidewalk, are lower than the waist of the average  
21 person, and with some of our inebriated guests, especially,  
22 it's a wonder to me we haven't had more of them just falling  
23 right over the bridges.

24 So I am assuming they would be a little higher,  
25 and, maybe, that was the reference you made --



1 MS. SANTER: Yes.

2 FATHER DURANTE: -- to some of the requirements no  
3 longer being met.

4 MS. SANTER: Yes. I believe -- Mike, correct me  
5 if I am wrong -- but I believe today's bridge with the  
6 railing would have to be, I think, 42 inches tall. And I am  
7 sure that the existing one is -- I don't know what it is, I  
8 didn't measure it, but I am pretty sure it is not 42 inches  
9 tall.

10 FATHER DURANTE: I would say so.

11 MR. COOPER: You're dead-on, Barb.

12 FATHER DURANTE: Very nice.

13 MS. SANTER: Thank you.

14 MR. ERNY: This is Greg Erny, and I am going to  
15 jump in because I am going to have to leave for another Zoom  
16 meeting. Much of what I have to say has already been  
17 discussed.

18 There was a goal to maintain the pedestrian  
19 access. My comment was going to be, hey, let's not maintain  
20 it, let's enhance it. And that's been discussed already, so  
21 I think -- there is a lot of foot traffic with and without  
22 cars on that bridge. So let's make sure that people have  
23 access to that park and to the island.

24 With respect to the lighting, my thoughts are the  
25 lighting is great, but it wants to be subtle, it wants to be

1 soft, it wants to be a non-evident source of sorts.

2 I don't want to look at the lights per say; I want  
3 to look at the area around it and not let the light sources  
4 be the thing cause glare and/or distractions from the  
5 natural beauty and features of that area.

6 From a practical standpoint, let's -- and I also  
7 prefer the single span without the intermediate support.

8 There are views from above the bridge, and there  
9 are a lot of views from below the bridge if you are a  
10 participant in that park; whether it's as a person wading in  
11 the water or fishing or whatever, and not having to look at  
12 a big bridge pilaster underneath, I think, would just  
13 certainly enhance that.

14 The LED lighting issue with the colors, today's  
15 LED lighting opportunities provide the ability to actually  
16 incorporate color change within those type of lights.

17 So whether you like to do it or not, want to do or  
18 not, LED light source does afford that opportunity.

19 The other thing I would ask is let's provide an  
20 opportunity for utilities to be to cross river somehow or  
21 another within the bridge.

22 MS. SANTER: Oh, yeah.

23 MR. ERNY: The last thing we need is a brand-new  
24 bridge with a new piece of technology that needs to cross  
25 the river, and we get a big conduit or pipe or something and

1 it get strapped on the outside of the bridge because it's  
2 the only way you can get it cross.

3 So see if we can find a way to afford some access  
4 raceway plantums (phonetic) or whatever I might be within  
5 the design of the bridge to afford paths for future,  
6 yet-to-be-known things that need to cross the river.

7 That's my comments. Thank you very much for a  
8 very nice presentation.

9 MS. SANTER: Thanks, Greg.

10 MS. BUJA: I wanted to make one more comment.  
11 While we were listening to your presentation, which really  
12 was great, I had no idea of what you were talking about when  
13 I first walked into this. I think it's fabulous.

14 I went back and looked at some pictures of some  
15 other bridges both modern, art deco, and some very old ones,  
16 16, 1700s.

17 One of the common elements on all of them that I  
18 personally found really, really attractive is that the  
19 lighting is on the outside of the bridge, and it frames the  
20 structure of the bridge and enhances the structure of  
21 bridge, as opposed to being underneath and attracting the  
22 eye to parts that I'm not really sure that's what we want to  
23 be emphasizing.

24 MS. SANTER: Do you mean like having  
25 pedestrian-scaled lights kind of on the corners?

1 MS. BUJA: No. No. This is on the frame of the  
2 bridge itself.

3 MS. SANTER: Oh.

4 MS. BUJA: The structure of bridge on the outside  
5 of it so that as you're walking down the street toward the  
6 bridge, you're aware of that beautiful architectural design  
7 without it slapping you in the face.

8 MS. SANTER: Gotcha. Okay.

9 MS. EBEN: Hello, everybody, this is Michon. I  
10 just had a comment. I do like everybody's comments, you  
11 know, lighted, not lighted, how we light it, I think that's  
12 all important.

13 But I really did like Gregory and John's comments  
14 about not too much lighting. I think as human beings,  
15 everything's about us, and we do need let the natural river,  
16 the natural surroundings have their nighttime. That's  
17 important.

18 But I do understand that people are under the  
19 bridge, they are going to the around the night. So that's a  
20 hard one for me. So I'm not there, or I don't have a vote  
21 for either. I just think that is going to be a big one.

22 My other comment is about back on slide number  
23 5 -- and you I don't have to go back to it -- it's the  
24 history, the site history, and I know that site history is  
25 just for the bridge only.



1           And, maybe, my question needs to be further --  
2 maybe my question and my comment can be in the actual NEPA  
3 document, or somehow we can mitigate down the road that  
4 there be a historical marker for the Truckee River and  
5 how -- because there is the history of the river and the  
6 native culture, everything has a connection.

7           So this bridge today has a connection to that  
8 river. And I know we're talking about the bridge and the  
9 theme, but I do want that make sure not to leave the  
10 indigenous culture out because the Truckee River was named  
11 after a Paiute -- a very great Paiute man.

12           And, actually, the pioneers that named it after  
13 him, they did get the name wrong. We never had a Paiute man  
14 named Truckee. The name was misinterpreted. I just think  
15 there needs to be some type of history on that.

16           Don't leave out the native culture. That's been  
17 happening far too long and everything -- when the native  
18 culture is left out, then it looks like we weren't here  
19 anymore.

20           There is a theme that has happened this whole year  
21 of don't leave things out. I think that's important.

22           If slide number 5 is going to be a public  
23 document, we really should put some culture in before the  
24 1900s, even though it's not about the bridge.

25           So I would appreciate that. We can talk about

1 that further, and that could be, like I said, in the NEPA  
2 document, the whole history of the Truckee River because we  
3 will be commenting on that.

4 That's my comment. Thank you.

5 MS. SANTER: Thanks, Michon.

6 MR. BREZINA: Hi, Barb. This is Matt with the  
7 City of Reno Parks Department. I had a comment on the  
8 bridge style.

9 I had originally preferred the single span -- it  
10 looked a little bit better, opened up the river -- until it  
11 was talked about how the precast girders would kind of angle  
12 down over the walk path that goes underneath the bridge.

13 I think that's a very important aspect of the park  
14 plan and the bridge plan, as it allows people to walk from  
15 east Wingfield to west Wingfield without having to cross the  
16 street.

17 And I don't think we want to restrict that if it's  
18 going cause us to have to block that path or, you know, I  
19 don't know if there are any height guidelines, but I think  
20 that should be taken into account.

21 MS. SANTER: Matt, can you clarify that? I am not  
22 sure I'm understanding. You said you used to like single  
23 pier, but now you're thinking the clear span with no pier is  
24 the better choice?

25 MR. BREZINA: No. Well, I think with no pier

1 would look better.

2 MS. SANTER: Okay.

3 MR. BREZINA: However, I don't want that to  
4 restrict the walking path that goes under it.

5 It looked like the precast girders that were going  
6 to be used for that single span had to angle down to support  
7 traffic, which would kind of minimize the head height on the  
8 walking path underneath the bridge.

9 MS. SANTER: Oh, I see. Okay. Gotcha.

10 MS. KOSKI: Barb, maybe somebody could speak to,  
11 just in general terms, we're looking at -- without a center  
12 pier, then what are we looking at for sidewalk widths and  
13 the rails, for example? Because the rails have to be  
14 traffic rated and such.

15 So do you have kind of a feel for what we're  
16 looking at sidewalk width-wise if we have no pier or with a  
17 pier?

18 MR. COOPER: Kerrie, I can answer that. Right  
19 now, the way the roadway's been configured in our  
20 preliminary layouts, it's eight-foot sidewalks across the  
21 bridges.

22 Those could be widened out to 12 feet or more,  
23 particularly with a center pier to provide support for that  
24 widen area.

25 The rigid-frame structure, it gets support its

1 support to hold up traffic -- Matt as you were  
2 questioning -- from the thickened ends, and that translates  
3 worst end into the abutments that way.

4           So, yes, it would be a deeper structure with a  
5 clear span, and it would be something that would need to be  
6 studied: How much head room that takes up versus the  
7 preferred elevation for the pathway underneath the bridge.

8           I'm assuming at flood state, that pathway is going  
9 to be inundated; but normal times, that pathway is going to  
10 want to be clear and open. So we can't oppress it too much  
11 to get under the bridge.

12           But it would be some part of our further studies  
13 on what the grades would actually look like and elevations  
14 of things and how much head room.

15           MS. SANTER: Good to know.

16           MR. COOPER: Matt, does that answer the question  
17 that you had?

18           MR. L'ETOILE: Kind of along those lines,  
19 Mr. Cooper, I was curious if you could get a ten-foot-wide  
20 sidewalk with a clear span?

21           MR. COOPER: Yes, it's possible. And anything is  
22 possible, it's just the idea that clear span was to try to  
23 minimize the depth added in over the river.

24           We can thicken that up a little bit to provide a  
25 deeper section to support the sidewalk and push the width



1 out to something wider than the standard eight foot that  
2 we've got on there right now.

3 MR. L'ETOILE: Okay. Thanks, Mike.

4 MS. SANTER: I mean, eight feet is actually pretty  
5 good for a sidewalk width because I think, like, standard  
6 commercial would be five feet. So eight feet is not bad  
7 with a clear span.

8 Great. Any other comments on these two topics? I  
9 really appreciate everyone's feedback.

10 MR. L'ETOILE: One other question, maybe, Barb,  
11 just thinking about pedestrian circulation.

12 If there is no grading in between the bridges,  
13 would there be an opportunity to look at the connections --  
14 just the point of connection between the pedestrian walks  
15 and the sidewalk along Arlington?

16 Nope. Judy's not cringing. Maybe she's  
17 covering --

18 MS. TORTELLI: I am going to jump in there a  
19 little bit. Right now, John, we don't know for sure what  
20 the footprint of our bridge is going to look like.

21 So once we determine how far out we're going to be  
22 going, then we'll start looking all connections into the  
23 existing pathways. But, I mean, access to park and to those  
24 existing pathways is a key component of this project.

25 So, like I said, right now were not sure. We're

1 kind of in the final stages of determining what our  
2 footprint is going to look like and how high do we have to  
3 raise that bridge and how far out are we going to be.

4 So, hopefully, that helps answer your question.

5 MR. L'ETOILE: It does. Thanks, Judy.

6 MS. TORTELLI: Okay. Well, we're going to go  
7 ahead and keep moving on. This is great feedback. Thank  
8 you, everybody.

9 Let's move on to our railings.

10 MS. SANTER: Okay. So one of aspects of design  
11 that we thought was important was to maintain -- to  
12 establish some element of transparency to the bridge  
13 railing.

14 As might recall, the existing bridge railing is  
15 solid concrete. So when you're driving in your car, you  
16 cannot see the river at all. I mean, certainly if you're on  
17 the bridge and looking over the railing -- although it's too  
18 short, so it particularly works today -- you can see the  
19 river.

20 But just work -- having been involved with  
21 aesthetics on both the Virginia Street and Center Street  
22 replacements over the years, it's been kind of a common  
23 request of the public to be able to have some element of  
24 transparency and be able to see the river.

25 So we're recommending that that should be -- that

1 is kind of an important aspect of the design. You can see  
2 the examples that we showed on the upper left is not a  
3 super-decorative railing, but it's got the transparency with  
4 the cutouts in the concrete.

5           The lower left is more of a hybrid design we just  
6 came up with that has vehicular rating of those two heavy  
7 rails, the horizontal rails, but then exterior railing and  
8 those bars are much more transparent. So it's kind of the  
9 best of both worlds. This was just a quick study we just  
10 did, but it kind of incorporates that little chevron  
11 elements that is on the Downtown Post Office and some of  
12 other -- it's like a kind of an art deco feature.

13           And then I will just go through the photos and  
14 then the list of topics here.

15           Then the photo on the lower-right side is one of  
16 the railings from Center Street Bridge, which is like a  
17 hybrid of both the concrete vehicular-rated railing with a  
18 transparent railing on top. So it's possible to do  
19 something like that as well.

20           But, again, just to summarize, this topic slide is  
21 we do recommend that people want to be able to view the  
22 river.

23           We do think in this case we should avoid the  
24 double railing concept because there are so many special  
25 events out here, and people really want to be able to walk

1 back and forth across the street because it's actually  
2 closed for special events.

3           So we think we should avoid that double railing,  
4 but we can still accomplish the transparent railing if we do  
5 something along the lines of the lower left or one of these  
6 other options.

7           Again, there is a potential for some decorative  
8 railing and decorative steel hybrid or a pedestrian --  
9 excuse me, a vehicular-rated metal guardrail rail with a  
10 custom artistic-detailed grading on the outside, and that's  
11 kind of like on the lower left.

12           And then on the lower right, the art deco,  
13 concrete railing exists along the norther floodwall, Sierra  
14 Street Bridge and Center Street Bridge.

15           So we have kind of got the -- kind of the nod to  
16 the art deco already down there on quite a few of these  
17 bridges.

18           So those are kind of the big ideas on this, is do  
19 you want the transparent railing? And then, do you agree we  
20 should avoid the double railing? And we'll have a  
21 discussion on this in a moment.

22           Again, this other topic is to maintain pedestrian  
23 accessibility. Again, in order to do that, we recommend  
24 avoiding the double-bridge railing because of the large  
25 number of special events and need for pedestrian



1 accessibility.

2           The photo on the upper left is the double railing  
3 example from Center Street right on the bridge where you've  
4 got that vehicular railing right behind the curb with the  
5 lights.

6           Then on the left, is the super transparent, but  
7 very pretty and sparkly, aluminum railing that we had to put  
8 the vehicular railing on the right side there because the  
9 one on the left does not stop cars from, possibly, falling  
10 off the bridge if somebody were to have to a mistake there.  
11 So we're recommending not doing this. We think we should  
12 have smooth pedestrian movement across the street.

13           Also, even if the elevation of the street needs to  
14 be raised, and to to kind of make sure that point is -- we  
15 don't have all the final answers on that yet, as Judy  
16 explained, but, again, looking at lower right little image,  
17 if Arlington Street does need to be elevated, we're kind of  
18 recommending that we just regrade those areas into the park  
19 and make it a little, you know, steepen up the grass areas,  
20 but don't put, for example, small retaining walls or things  
21 like in if we can avoid it.

22           Then we can use that opportunity to, maybe, do  
23 some street tree planting if the evaluation needs to be  
24 raised.

25           Then the last topic, not exactly in the

1 highlighting of this slide, but provide some graffiti  
2 coating for easy maintenance and graffiti removal.

3           The lower-left slide also is just kind of an  
4 example of streets that have a lot of pedestrians on them.  
5 That's kind of a plaza, but just the idea that the street  
6 could have trees and a few more trees, and it's closed off  
7 now and then, so it becomes kind of a really nice pedestrian  
8 space.

9           Okay. The third point in this series is the  
10 widened-bridge deck, and we already had a lot of discussion  
11 about. The consideration would be, do you want to widen the  
12 bridge deck to allow for greater pedestrian viewing?

13           And as Mike kind of pointed out, it looks like the  
14 single-pier-in-the-river type of bridge would need to be  
15 used in order to accommodate that.

16           Again, just pointing out that both Center Street  
17 Bridge and Virginia Street Bridge have those widened decks.  
18 You can see that in plan view, that's the Virginia Street  
19 bridge there, and then the Center Street Bridge; both the  
20 centers are kind of widened.

21           The photo on the lower left is a photo of that  
22 double-bridge railing with the super-transparent outside  
23 railing, and then the vehicular-rated inner railing.

24           Then the photo on the lower right is the Virginia  
25 Street Bridge with some people walking right about where the

1 bridge gets about as wide as it gets. It does cast a really  
2 nice shadow, I might say, on that photo.

3 So going --

4 MR. COOPER: Hey, Barb?

5 MS. SANTER: Yes.

6 MR. COOPER: Can I just make one point here on the  
7 wider sidewalk?

8 MS. SANTER: Sure.

9 MR. COOPER: Speaking in terms of it being a  
10 little bit more complex on the rigid frame structure to have  
11 the wider sidewalk, it's all about having the wider sidewalk  
12 just out at mid-span over the river.

13 Certainly, if we were to look at a wider sidewalk  
14 over the full length of the structure, that's a different  
15 animal altogether, and could be easily accommodated.

16 We need to be looking at how that wider structure  
17 ties in at both ends of the bridge. Does it match, does it  
18 fit in well, or does it cause other conflicts? We need to  
19 look to that.

20 That would be a way to get a wider sidewalk with  
21 that rigid-frame structure.

22 MS. SANTER: So just making the whole thing wider  
23 from one end to the other, you think that could be  
24 accommodated with the clear span?

25 MR. COOPER: Yes.

1 MS. SANTER: Oh.

2 MR. COOPER: Because you would be widening the  
3 structure underneath out as well. You wouldn't be  
4 cantilevering the wider structure off of a narrower bridge  
5 to create the effect just over the river.

6 But, as I say we'd need to look at how the tie-ins  
7 work at the ends of the bridge.

8 MS. SANTER: Yes.

9 MR. COOPER: For the structure, you have to make  
10 sure things fit together with what you're landing on at the  
11 two banks; that works together.

12 MS. SANTER: Great. Good to know.

13 MR. COOPER: Just a clarification.

14 MS. SANTER: Yes. Absolutely. That's really good  
15 to know.

16 Okay. So any questions? What are your thoughts  
17 about this? Again, the smooth pedestrian movement idea,  
18 railing or not double railing, should the railing be  
19 transparent, and should the bridge deck be widened.

20 So opening up to your comments on those topics.

21 MS. DOWTY: Hi, everyone. This is Kayla Dowty. I  
22 am representing the Carson-Truckee Water Conservancy  
23 District. We are the local sponsor for the 408 permit that  
24 this project is going to need.

25 I know that I am probably starting to sound like a



1 broken record on these working groups, but this has been a  
2 really nice presentation to kind follow the trends of the  
3 other bridges, and how access from those bridges is,  
4 especially for large equipment.

5           So just looking at these two bridges that you have  
6 here, the Virginia Street Bridge, while it's really pretty,  
7 these cantilevered sidewalks and then also the cable  
8 railing, allows virtually no access to the river from the  
9 bridge.

10           And I don't if any of you have seen it before, but  
11 I was just on a field walk with a couple of the guys from  
12 the City of Reno, and we were talking about, during a flood,  
13 both the City and the Carson-Truckee actually has  
14 contractors that bring large equipment, particularly like an  
15 excavator, out on to some of these bridge decks, and they  
16 actually pick logs out of the river as they're coming  
17 downstream to help mitigate flood impacts to the Downtown  
18 area.

19           So, obviously, like, the Virginia Street Bridge,  
20 there is no way that we can get access there to do that.  
21 Then, also, the Center Street Bridge with the double  
22 railing, there is no way to get access there either.

23           So we're quickly kind of like ticking off bridges  
24 where you can have access during floods, also just general  
25 maintenance. I know the City of Reno needs to be able to

1 access the bridges for general maintenance.

2 So that is going to be really, really important  
3 when the Carson-Truckee is reviewing the application for the  
4 408 permit, that there is access for large equipment.

5 One other thing that I'll mention is that, since  
6 this one is just adjacent to the kayak park, due to the  
7 flows through the kayak park, they tend to get sediment  
8 buildup in the kayak park features, and as, just a general  
9 requirement of the Martis Creek Agreement, the City of Reno  
10 has to keep that sediment out so that we don't raise water  
11 surface elevations.

12 So as a result of that, it would also be really  
13 beneficial, I think, to the City in general to have another  
14 way to access the river, not even from the bridge, but also  
15 just from the riverbank, again, with large equipment.

16 MS. SANTER: Thank you.

17 MS. DOWTY: So I guess to just kind of round out  
18 that comment, I don't think that double railing would be  
19 preferred, at least from our standpoint.

20 Then, also, I am hoping that the design is not to  
21 encompass these cantilevered sidewalks like the Virginia  
22 Street Bridge did.

23 I don't know if you guys are at the point in the  
24 conceptual design that you've, I don't know, been able to  
25 kind of identify what access is, or what you are planning

1 access.

2 MS. TORTELLI: So, Kayla, I really appreciate your  
3 bring up the flood debris removal. That has been a key  
4 component --

5 MS. SANTER: Um-hmm. Yes.

6 MS. TORTELLI: -- and something that we do know  
7 and understand that has to carry forward.

8 But you're right, we start talking about pretty  
9 bridges and making them pretty and making them wide, and  
10 sometimes we lose site of that access.

11 So, like I said, I appreciate your bringing that  
12 up.

13 At this point, again, as we start to to determine  
14 what our footprint is going to be and look a little bit more  
15 closely into the access, we're just going to have to keep  
16 that in mind.

17 We're not quite there yet, but it's coming.

18 MS. DOWTY: That would be great. Thank you, guys.  
19 I appreciate it.

20 MS. TORTELLI: Um-hmm.

21 MS. JONES: Judy, this is Theresa Jones. Can you  
22 hear me?

23 MS. TORTELLI: Yes.

24 MS. JONES: So just along those lines with regards  
25 to access -- and NDOT can speak for this, and I've said it

1 before -- the very large UBIT, under-bridge inspection  
2 trucks, need to have access to inspect the underneath of the  
3 bridge and the girders.

4 So these wider sidewalks makes that difficult, and  
5 I know that you guys are keeping that in mind. I just  
6 wanted to piggyback off of Kayla's comments.

7 Additionally, I would assume that this is a  
8 brand-new bridge, and that it's being modeled and designed  
9 to pass the hundred-year flood event, along with taking into  
10 consideration the impacts to debris and scour and what not.

11 I'm certain that that's being considered in the  
12 design of this bridge.

13 MR. COOPER: Yes, Theresa, absolutely.  
14 Absolutely.

15 And that was part of -- maybe, it was not real  
16 clear, but part of my comment on the thinner structure  
17 having less of an impact, potentially, on profile on  
18 Arlington, has to do with making sure the underside of the  
19 bridge is high enough to pass flood flows and considering  
20 debris.

21 The thinner structure might do better at providing  
22 that flood capacity and have less impact on profile on the  
23 roadway above.

24 We're pretty constrained at both ends of the  
25 project; the north bridge ties into an intersection, the



1 south bridge does as well.

2 So there is little we can do there without profile  
3 without stepping on adjacent properties.

4 So that is definitely something that we'll be  
5 looking more closely as we move ahead.

6 MS. SANTER: Great.

7 MS. BUJA: I'd like to add one comment in terms of  
8 double railing versus single railing.

9 That bridge on Virginia Street is so beautiful, and  
10 it's a statement in and of itself. It makes this marvelous  
11 statement.

12 I think staying with a single railing helps to set  
13 that bridge off as its own entity, and then everything kind  
14 of works out from it as aesthetic support.

15 If we keep with a lower profile on those other  
16 bridges, in terms of their railings, but I'm so glad you're  
17 going to open up the railings visually. That's going to be  
18 cool.

19 MS. SANTER: Great. Thank you.

20 MR. L'ETOILE: I would agree. The transparency  
21 is -- I don't think that should even be a question. That  
22 should be viewed through, for sure.

23 With the double railing, the one design solution,  
24 if you're trying to get safety for both pedestrians and  
25 safety as it is a roadway as well as, maybe, not have a

1 solid wall, but some aesthetic type of pilaster or bollard  
2 so that pedestrians can move back and forth easily, but a  
3 car couldn't come up on to the sidewalk.

4           Then the widening, I think pedestrian circulation  
5 is key. If there's a desire to have like a viewpoint or  
6 someplace to gather for people along the bridge structure or  
7 directly over the river, maybe it is closer to the abutment  
8 so there's, you know, bulb out on all four corners or two  
9 with corners that people can look out over to water at those  
10 sections at the more-structurally robust portions of the  
11 bridge structure.

12           MS. SANTER: That's an interesting thought.  
13 Great. Any other comments those topics?

14           Okay. So maybe we can move on to our final couple  
15 of topics.

16           One is textured abutment walls. So what we were  
17 thinking of considering an abutment wall, which is the wall  
18 that is right underneath the bridge, as opposed to the  
19 floodwall, just to clarify, that might have some more  
20 texture to enhance the pedestrian experience and river-user  
21 experience below the bridge. And, again, this is the only  
22 bridge right now that has pedestrian access below the  
23 bridge, but, currently, the abutment walls are smooth.

24           A couple things we were thinking about, just  
25 brainstorming, would be there is already cobble texture

1 that's being used, like on the Riverwalk, and, actually,  
2 some of the more -- I hate to use the word "historic," but I  
3 would say the oldest floodwalls down there do have sort of  
4 this cobble texture.

5           So the image on the lower left is intending to  
6 represent that texture, but there, of course, a myriad of  
7 possibilities.

8           So the second photo from the left on the bottom is  
9 actually a concrete texture called a "form liner" where that  
10 was just a custom texture that was developed for a certain  
11 project and is super stylize, and I think it's really  
12 pretty. I wouldn't say we would do this exact one here, but  
13 that's another possibility.

14           The third one from the left is actually intended  
15 to kind of represent a cottonwood tree bark, so that really  
16 furrowed bark. That would be a real naturalistic  
17 possibility.

18           Then the one on the right is just like showing  
19 what the Greek key looks like. That's an element -- real  
20 stylized element from the Downtown Post Office project.

21           So I guess it's kind of a two-part question. I  
22 mean, we would certainly explore other textures, but what do  
23 you think about having a texture, I guess, versus just  
24 smooth concrete, which is what's there now?

25           So we will go through our next couple topics, and

1 then have feedback on that here in a minute or so.

2 I might go back to that and -- well, actually, I  
3 can just talk about that here as well.

4 So, similarly, the floodwalls -- this project is  
5 not about replacing a lot of floodwall.

6 However, the photo on the lower left kind of shows  
7 you the north floodwall. That is a picture of the north  
8 Arlington Bridge, and you can see there's a kind of a smooth  
9 piece of wall where that culvert is, then you go to this  
10 kind of older, rock type of floodwall, but then it  
11 transitions into smooth concrete. So we do have a variety  
12 of floodwalls in the Downtown.

13 The diagram on the upper left is really the most  
14 common type of floodwall on the north side of the river,  
15 which is just smooth concrete, and then it's got the, kind  
16 of, pilasters that extend down the wall -- actually, I think  
17 may not be completely accurate. I think those pilasters,  
18 the concrete extends even further down the wall than what is  
19 shown there -- then it's got the globe, kind of, art deco  
20 lights on them.

21 So, I guess, we're kind of saying that we think  
22 the floodwalls, there should be some consistency between the  
23 bridges. That's the first topic.

24 Secondly, the textures, horizontal reveals open  
25 concrete railing and pilasters extended below, plus the



1 lighting, we kind of think should be consistent so it  
2 doesn't look like so much of a hodgepodge.

3           Maybe the floodwall on the south side of the  
4 bridge should match with the cobble along the Riverwalk.  
5 That's a possibility on the south bridge replacement.

6           Then, I don't know if we can do this, it would be  
7 great if we could do some kind of narrow planting at the  
8 base of the wall to soften the height.

9           You can see, like on the photo on the lower left,  
10 where the big riprap was added into the channel at the edge  
11 of the wall. That was, I'm sure, to facilitate the kayak  
12 park. It's kind of on the right side of the photo, and now  
13 you can see some trees have grown in there.

14           So, of course to me, that is nicer looking than no  
15 trees. However, from the engineering or maintenance side,  
16 maybe that is not the most ideal scenario.

17           Then just on the right-side photo, that's actually  
18 a photo of the new and old floodwall at the Post Office. So  
19 that was constructed as part of the Virginia Street Bridge  
20 Project, and in this case, the wall has the horizontal lines  
21 or reveals, which does break up that big, expansive concrete  
22 kind of nicely.

23           So I think the question is on the floodwalls,  
24 we'll come up, do you agree that there should be consistency  
25 between the bridges?

1           If there is, on the north side, it should probably  
2 look kind of like the upper left, and in the south side, it  
3 might want to look like the cobble that's at the Riverwalk.

4           So maybe move on to the last slide, and then we  
5 can open up for final discussion.

6           So the final topic is the idea of, kind of, the  
7 plaza street, and the consideration would be should we look  
8 at the Arlington Avenue plaza street concept to unify the  
9 park areas if the street between bridges needs to be  
10 replaced?

11           So the thought is to consider, number one,  
12 unifying, kind of, the paving so there's some continuity  
13 between sidewalk and the street surfacing. We can even  
14 consider using some permeable pavers on the street and the  
15 sidewalk to create a seamless transition and provide the  
16 storm water infiltration.

17           So that would be sort of represented by -- I mean,  
18 one version of that is on the lower-right side, that is a  
19 type of permeable paver that actually happens to exist on  
20 the northwest corner of the Arlington Street Bridge now. Of  
21 course, there many, many styles that could be considered.

22           The possibility is also to use like a sidewalk  
23 paving that has a unique stamp or a sand blast that kind of  
24 creates a theme. The photos on the two lower left are  
25 examples of that.

1           So on the lower left, that's actually a photo of  
2 the La Rambla, a super-famous street in Barcelona, where  
3 there is actually a canal underneath that street, and now  
4 it's a pedestrian plaza only. But the wave pattern was  
5 chosen because there is actually water. There is still a  
6 canal under there. So what was sort of a reference to the  
7 water that was once there, which I think is really  
8 beautiful.

9           Then middle one is just showing another design  
10 just using a simple, it looks like, sand blast or exposed  
11 aggregate.

12           So the big question here is just what do you think  
13 about the idea of, perhaps, creating this kind of plaza  
14 street idea, if the street between the bridges needs to be  
15 replaced?

16           So moving on to the final slide, it would be --  
17 these are the final, kind of, topics that we would like your  
18 feedback on.

19           One, again, would be the plaza street, and then  
20 the other one is should we incorporate some kind of texture  
21 on the floodwalls, and then on the -- or excuse me, the  
22 abutment walls. Should we incorporate texture on the bridge  
23 abutment walls? And then the floodwalls, should we maintain  
24 some consistency between the design of the floodwalls to  
25 what's there now?

1 So opening it up for more comment patient.

2 MS. KOSKI: So this Kerrie, again. Kerrie Koski,  
3 City of Reno, City Engineer. I am going to weigh in a  
4 couple of things.

5 The plaza street, I would like to say that  
6 Arlington Street is a street first. Please keep that in  
7 mind.

8 I would also like to say that accessibility to me  
9 is important to everyone. So I would like the group to  
10 think about -- textured surfaces and such are nice. They  
11 are fancy. They catch your eye.

12 But we, at the City, often get complaints about  
13 the smoothness, or if you think about people who are vision  
14 impaired, how to do they -- how do they navigate it? It's  
15 kind of a balancing act. So I am not really in support of a  
16 lot of fancy textures on a plaza -- in a plaza.

17 As far as the floodwalls go, low maintenance.  
18 That's all I ask is, whatever we put in there, let's make  
19 sure that it does, obviously, match -- I would support  
20 matching with adjacent, and make it low maintenance.

21 That's my two cents.

22 MS. SANTER: Gotcha.

23 MS. KOSKI: That's from the boring engineer. I'm  
24 sorry.

25 MS. SANTER: Appreciate that.

1           Anybody else?

2           MR. BREZINA: Yes, Barbara. This is Matt with the  
3 City of Reno Parks again. I agree with Kerrie. I think  
4 simple is better. I think some of the decorative textures  
5 look nice, but once they are covered in our concrete gray  
6 paint to cover the graffiti, they don't look very nice at  
7 all.

8           I think whatever is easiest to cover up and  
9 simplify the coloring schemes, I think would probably be  
10 easiest for everybody.

11          MS. SANTER: Any others?

12          MS. BUJA: This is Ann again. I don't know if  
13 it's possible, I always look at these sorts of things and  
14 think what are we going to think when we look this a hundred  
15 years from now? Will it feel timeless, or will it feel like  
16 you can point at it and say, oh, I know exactly when that  
17 was done.

18          And the first time I saw it, I really didn't like  
19 it, but the sculpture walls that are done along the highways  
20 now -- we moved here from Colorado, and on a lot of walls  
21 lining the highways, the sound barriers, we have the front  
22 range, we have geostructures that show how the land  
23 developed. When we moved here, you know, there are the  
24 quail along the highways.

25          So what if we used this as a place where we could



1 do flora and fauna? Local flora and fauna, and have that be  
2 the design along those flood walls.

3 I think it can be kept simple paint-wise, I think  
4 it's not too hard to maintain. Just a thought.

5 MS. SANTER: That's good. Well, I am from Fort  
6 Collins, originally, and I am very familiar with, like, a  
7 lot of the textures they did on the I-25 on the T-Rex  
8 Project that have, kind of, the beautiful buffalo and  
9 swallows.

10 I mean, John L'Etoile is in charge of the program  
11 at NDOT that does all that work for our highways.

12 So we are kind of knee-deep in that kind of work  
13 and reference. So we're all as excited, or at least I am,  
14 about that kind of thing.

15 I like, personally, the idea of, maybe, like,  
16 referencing the cottonwood or the trembling leaves somehow,  
17 because that was such a -- it's just such a significant  
18 component of the Truckee Meadows is, you know, were in this  
19 kind of funny oasis in the middle of the Great Basin, of  
20 course at the toe of the Sierra too.

21 John?

22 MR. L'ETOILE: Yes. I am part of that program,  
23 but those designs are yours, Barb. Those quail.

24 MS. SANTER: That's true. The quail and the fish,  
25 which I was referring, by the way, to the Lahontan cutthroat

1 trout that I am super excited about possibly returning to  
2 Downtown river now that they have been taking away some of  
3 the impediments like to the Derby Dam and all that.

4 I mean, apparently, there used to be 20-pound fish  
5 getting drawn out of the Truckee River in Downtown Reno. It  
6 is crazy.

7 MR. L'ETOILE: Yes. That's fantastic.

8 MS. SANTER: And that would tie in, maybe to some  
9 of the thoughts Michon had too about just referencing some  
10 of the importance of the Truckee River to the Tribes, you  
11 know.

12 Doing it in a really clean, timeless way, that  
13 would be my only concern is that -- and I know there is a  
14 way to do it, but just making so it's something that you  
15 kind of get, but it's kind of subtle and just beautiful and  
16 not -- just really --

17 MS. BUJA: Not tacky.

18 MS. SANTER: Not tacky. Yes. Right.

19 MR. L'ETOILE: Yes. Yes. If it's not completely  
20 literal, I mean, it's a little more abstracted so it  
21 resonates with the river and the cultural and the history of  
22 the Tribes, which I think would be great to have that thread  
23 in there this as well.

24 MS. BUJA: I grew up in New Mexico, and a lot of  
25 the architecture in Albuquerque and around pulls from the

1 Hopi and the Navajo and the Pueblo Tribes, and they are art,  
2 and it's replicated. It was really -- I mean, that was our  
3 art deco.

4 I am wondering if we can do the same thing again  
5 to reference the Paiute people. I mean, they've only been,  
6 you know, 800 -- as one of the women informed me: We've  
7 been here about 800 hundred years longer than you. And I  
8 look really great.

9 So if there would be a way to use that on the  
10 walls. Except I don't want them down on the water, I want  
11 them up where we can see them, always.

12 MR. L'ETOILE: Yes. A couple of other opinions on  
13 the patterns that you have there on the bottom.

14 The Greek patterning, I think is more -- resonates  
15 a little better with an architectural element. Maybe steel.  
16 Maybe no not so much for river.

17 And the -- I like the idea of the bark and  
18 bringing in the trembling leaves idea somehow. But bark  
19 being, you know, the patterning along that wall, just not  
20 sure about that.

21 I am a creature of consistency, so I think I'm,  
22 maybe along the lines with Kerrie on keeping the walls  
23 similar throughout the river corridor.

24 But the texturing does definitely impedes some of  
25 the graffiti. There's so much less graffiti, we find, on

1 walls that have a deeper texture and pattern to them than  
2 otherwise. So that would be important.

3 MS. SANTER: Thank you.

4 Anyone else?

5 MS. TORTELLI: Okay. Well, great discussion. I  
6 mean, I kind of have this slide here for group discussion  
7 and consensus.

8 I think, for the most part, all of the items that  
9 we have kind of proposed as aesthetic elements are important  
10 for us to carry forward. That's what I'm getting of the  
11 discussion, and we will carry all of those forward. We need  
12 to look at them in a little bit more detail.

13 I do want to remind the group that this is the  
14 feasibility study, so we're not really getting into the  
15 finer details of the aesthetics, but as we kick off NEPA and  
16 design, we will be.

17 So right now in terms of next steps, I mean, the  
18 group is going to put together a summary of notes from our  
19 TAC meetings and our three Stakeholder Working Group  
20 meetings that we have had with all of you, and present the  
21 findings and the comments to the City of Reno Council and  
22 our RTC Board.

23 Then we're going to refine some of our renderings  
24 and the way the bridge concepts look, and put together some  
25 of these aesthetic elements on the bridge so that we can

1 show that stuff to the public and get feedback from them at  
2 our second and final public information meeting.

3           Once we get some feedback from the public, we'll  
4 take that discussion and those comments and we'll gobble  
5 them up into the feasibility study and present to the City  
6 of Reno Council and RTC Board again, and then we will  
7 finalize things.

8           So I really appreciate everybody's participation  
9 today, and I think we've got great feedback.

10           Like I said, I feel like as a group, all the  
11 things that we have presented today, everybody, you know,  
12 for the most part, we should carry everything forward.

13           I think our recommendation to remove the double  
14 railing -- I mean, not look at the double railing, I feel  
15 like that's something that we don't need to carry forward  
16 because it limits pedestrian accessibility.

17           MS. SANTER: Um-hmm.

18           MS. TORTELLI: And I'll just take a breath and see  
19 if anybody disagrees with that.

20           Okay. Are there any additional ideas that anybody  
21 wants to throw out in the last -- we don't really have any  
22 minutes left, but in the last little bit of time that we're  
23 here?

24           MR. L'ETOILE: If you eliminated the curb so you  
25 don't have a -- maybe, have a different patterning for the



1 pedestrian and the vehicular.

2 MS. TORTELLI: Remove to curb. I guess I am not  
3 following.

4 MR. L'ETOILE: Well, sorry. You know what, I was  
5 thinking that in terms of if we the double railing or some  
6 type of staggering railing, we might do something like that.

7 But, nope, scratch that, Judy.

8 MS. TORTELLI: Okay.

9 MR. STEWART: Judy, this is Brian with RTC. I  
10 really like the idea that Mike brought up with respect to  
11 the wider bridge. And, maybe, at least looking at is it  
12 feasible, since it's a feasibility study, to provide that  
13 from abutment to abutment. Then on that surface, you can do  
14 (inaudible), so you might not have to use everything for  
15 pedestrian or bikes or transportation, if you can  
16 accommodate that.

17 So I think that would be something to look at that  
18 could then give you some of these other options out there.

19 So, in any case, I don't want to lose that idea to  
20 explore a little bit.

21 MS. TORTELLI: Okay. Anything else from anyone?

22 MS. HARSH: Judy, this is Toni. Do you want the  
23 comments from the public?

24 MS. TORTELLI: I certainly -- Toni, you are more  
25 that welcome to speak now.

1 MS. HARSH: Okay. And thank you so much. I  
2 thought it was an amazing presentation. I loved everything.

3 And what I did the last time, was I simply went  
4 through some highlights on my notes, and you captured them  
5 beautifully, and I just thank you so much for letting me  
6 participate in this.

7 So very quickly, okay -- because I have seven  
8 pages of notes, but I only pick up a few -- some of the  
9 things that I wanted to bring up is don't -- which may  
10 follow up with -- or come up with Parks and Recreation, is  
11 there has always been a huge appetite for the ice rink, and  
12 that seemed to be best location down there. And that's  
13 later on, you know, just a comment. That seemed to be --  
14 and that's why we had that few-mile-an-hour thing is it's  
15 still residue from the ice rink.

16 Regarding street lights, we have a tendency, and  
17 the example is perfect on Booth Street, that we somehow  
18 order our lights and forget that sometimes they are going on  
19 pedestals and not at street level, so we get them out of  
20 portion to the pedestrians. So I just would remind us of  
21 that. I think you can see examples of that through the  
22 pictures.

23 I love the idea of globes, and, by the way, I love  
24 the green lights in the lights down on the river. It's very  
25 exciting to see.

1           Regarding that floodwalls, having participated in  
2 the conversations with the natural river and flood  
3 management, the floodwalls are a part of that. I don't know  
4 where that falls with funding right now, but discussion on  
5 floodwalls goes clear back into the 1990s.

6           I really like the idea, and I don't know who  
7 mention it, is the idea of a certain motif on the north side  
8 and a different motif on the south side, because you don't  
9 feel like you're going down a canyon and it's repetitious.  
10 Plus the necessity for floodwall is far higher on the north  
11 side than the south side.

12           Debris. Debris is paramount, I mean, you've heard  
13 this all the way through. That is the main discussion that  
14 comes out of the flood management, and we're still not  
15 addressing that upstream where it should be addressed like  
16 they do in other communities. So that's a separate  
17 conversation that needs to be addressed and needs  
18 established because that will clean up a lot of the  
19 situations that you have with the structure of the bridges  
20 Downtown. And the need for grass and the timbers and the  
21 trees out of that.

22           So I would just comment I don't know you want to  
23 throw that little monkey-on-your-back to somebody to talk  
24 about debris sweep.

25           Just a point of discussion with all of the

1 components of where you are going into river for  
2 pedestrians. There is an ADA accessible access, which is on  
3 the south side of the river across from Barbara Bennett  
4 Park. So you might want to put that on your map that.

5           The -- let me see. Oh, I loved the comments that  
6 were made about that the lighting should not be  
7 overwhelming; that it should be enhancing. I think that  
8 that came back around many times, and I think there's going  
9 to be a happy medium there of where it is going to be to  
10 enhance and where it's going on, you know, some sort of --  
11 we don't want dark shadows, but you guys know that better  
12 than I do.

13           Believe it or not, I'm on page 5.

14           Yes. Most important coming out of the workshops  
15 from living river, which go way back, is the desire for the  
16 community to interact with the river.

17           So I think you've captured that, but just  
18 reiterating that the public is just so involved in that and  
19 excited about it that has not waned, even though we have had  
20 to change municipal code so that we at least allowed people  
21 to get into the river. So that was pretty exciting.

22           Regarding your accessibility for pedestrians, they  
23 have changed the shading requirement down along the river.  
24 There used to be a shading -- you know, you couldn't be  
25 above a certain elevation on the north side of the river at

1 a certain time. That has changed, and it may not impact --  
2 well, it could -- not it won't impact -- the Arlington  
3 Street Bridge. But it's just a comment that we do have a  
4 lot of slippery -- very slippery situation as we come down,  
5 coming from south to north entering Arlington Street Bridge.  
6 You know, who knows what we're going to have in the future,  
7 but that was always a major safety issue there.

8 That cover it in no particular order. I love  
9 everybody's comments. They were so valuable. Thank you for  
10 listening to me.

11 MS. SANTER: Thank you.

12 MS. TORTELLI: Toni, thank you so much for that  
13 input. That was really helpful. And I appreciate you being  
14 patient and waiting until the end.

15 MS. HARSH: Sure.

16 MS. TORTELLI: Thank you for that.

17 MS. JONES: Judy, this is Honor. May I also make  
18 a couple of comments?

19 MS. TORTELLI: Sure, Honor. Go ahead.

20 MS. JONES: I missed your last presentation, so  
21 some of these -- my comments will revert back to that.

22 I do like your presentation and mentioned the  
23 first 100 years, and also bringing it into the coming 100  
24 years and how the use of the river and how we're going to  
25 enjoy it as a population is going to be quite different.



1           With that in mind, and you said it very  
2 accurately, we've got a street (inaudible) and I would like  
3 to come back to one thing because I didn't quite get the  
4 feasibility for why we dropped out an elevated bridge  
5 because everything I'm hearing today kind of tells us that  
6 an elevated bridge would eliminate some of the issues that  
7 you have discussed.

8           One would be to start out with accessing for  
9 maintenance, and the other would be debris. We wouldn't  
10 have that element.

11           We've also got a repetition of the Virginia Street  
12 bridge with an elevated look, and we would again accommodate  
13 the upcoming and fastly growing -- and this is popular  
14 across America -- outdoor activities at Wingfield Park.

15           We know with the population increase here in the  
16 Reno area, we're only going to get more people that will be  
17 attracted to come down there for more special events.

18           I am wondering if we are -- we don't need to  
19 reconsider some of these things.

20           I would like to then kind of hit on something I  
21 have dealt with in the last 15 years of my life because I  
22 have taken care of an ADA needs person, but I can't stress  
23 enough to keep your designs on your flat surfaces to be  
24 ADA-compliant so that you just don't have any niches or  
25 grooves that your wheels can get stuck in.

1 Remember, too, about the railings. And,  
2 incidentally, I know that they increased the heights of  
3 railings, but let's be careful not to use your designs to be  
4 a ladder so that somebody could climb up them and go over.

5 I like the lighting because it is increased. It's  
6 a modern trend. But I don't see us as Chicago. Chicago did  
7 show the string of lights, but that mimics the skyscrapers.

8 We've got the curve of the park, the curve of the  
9 river, so I believe our lighting should duplicate that sort  
10 of look.

11 But I do want that say, or ask this question is,  
12 maybe, it should be reconsidered to look at the elevation  
13 again of a bridge. And particularly for safety.

14 We've got ambulances that need to go to St. Mary's  
15 fast; we don't need to be caught with road closures or  
16 people crossing the street. We've got fire trucks, we've  
17 got all of those, and I know there is a little engineer in  
18 my background, but still function before form is a rule.

19 So with that, I think I've contributed a citizen's  
20 comments. Thank you. It was just a very well put together  
21 presentation, and I know those things take time to do, so  
22 kudos to all who were work concerned. Thank you.

23 MS. SANTER: Thank you.

24 MS. TORTELLI: Yes. And thank you, Honor. I'll  
25 have to reach out to you. We have a recap from our last

1 Stakeholder Working Group meeting where we talked about the  
2 reasons behind why the elevated bridge concept was not  
3 recommended to be carried forward. There's also some more  
4 details on that in our TAC-2 meeting stuff.

5           When I get that stuff posted on the website, I  
6 will reach out to you and let you know so you have an  
7 opportunity to look at that.

8           MS. JONES: Thank you.

9           MS. TORTELLI: We will be, like I said, right now,  
10 the way this process has worked is we started off with a  
11 public information meeting and we went out to the public and  
12 we said: Hey, these are our bridge concepts that we want to  
13 look at.

14           We told the public about the process that we were  
15 planning to follow, you know, our TAC meetings and our  
16 Stakeholder Working Group meetings.

17           We're through those now. We have some recommended  
18 bridge concepts that we have to take back to the public and  
19 get their input.

20           So we'll be presenting that information to the  
21 public, probably, early next year, maybe, like February or  
22 March. So stay tuned for that.

23           Well, we're not doing too bad. It's about 15  
24 minutes after 3 it looks like. So I think we got through a  
25 lot of the material in a short amount of time.

1 Christmas is next week so I wore my Santa hat  
2 today. I just want to try and stay in the -- get myself in  
3 the Christmas spirit here, and I would like to wish  
4 everybody happy holidays.

5 Thank you for participating.

6 MS. SANTER: Thanks, everyone.

7 (Meeting was concluded at 3:14 P.M.)

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1 STATE OF NEVADA )  
 ) ss.  
2 COUNTY OF WASHOE )

3

4 I, BRANDI ANN VIANNEY SMITH, court reporter, do  
5 hereby certify:

6 That I was present via Zoom audio visual on  
7 December 15, 2020, at the RTC Stakeholder Working Group  
8 Meeting-3, and took stenotype notes of the proceedings  
9 entitled herein, and thereafter transcribed said proceedings  
10 into typewriting as herein appears.

11 That the foregoing transcript is a full, true, and  
12 correct transcription of my stenotype notes of said  
13 proceedings consisting of 86 pages.

14 DATED: At Reno, Nevada, this 26th day of  
15 December, 2020.

16

17 /s/ Brandi Ann Vianney Smith

18 BRANDI ANN VIANNEY SMITH

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